



VILLAGE OF WAUNAKEE / TOWN OF WESTPORT

JOINT PLAN COMMISSION

Town of Westport Community Room

5387 Mary Lake Road

February 12, 2019 6:00 PM

Agendas may change prior to the commencement of the meeting.
Please check the posting board at the Village Hall, 500 W. Main Street,
Waunakee, Wisconsin for the current agenda.

AGENDA

CALL TO ORDER

ROLL CALL: Ken Sipsma, Dean Grosskopf, Eileen Bruskewitz, Brad Zeman, Brian Malich, Tim Black

MINUTES

- 1 September 11, 2018**
- 2 December 11, 2018**

PUBLIC COMMENT

NEW BUSINESS

- 1 Public Hearing and Discussion/Action on a Request by Drumlin Ridge Farm Vineyards LLC to Amend the Existing Conditional Use Permit for 6000 River Road**
- 2 Public Hearing and Discussion/Action on a Request by Wisconsin Scaryland to Extend the Original Conditional Use Permit for County Highway M for an Additional Two Years**
- 3 Westport Commercial Development Design Guidelines Discussion/Recommendation**
- 4 Status Discussion on Woodland Crest and HyVee Development**



ADJOURN

Any person who has a qualifying disability as defined by the Americans with Disabilities Act that requires the meeting or materials at the meeting to be in an accessible location or format should contact the municipal clerk at (608) 850-8500, 500 West Main Street, Waunakee, Wisconsin, at least twenty-four hours prior to the commencement of the meeting so that any necessary arrangements can be made to accommodate each request.

VILLAGE OF WAUNAKEE BOARD AND PLAN COMMISSION MEETING TOWN OF WESTPORT BOARD AND PLAN COMMISSION MEETING

Notice is hereby given that the Village or Town Board or Village or Town Plan Commission may attend this meeting. No action will be taken by the Village or Town Board or Village or Town Plan Commission at this meeting.

Next scheduled meeting is 3/12/2019 6:00:00 PM.

**VILLAGE OF WAUNAKEE/TOWN OF WESTPORT
JOINT PLAN COMMISSION MEETING
500 West Main Street
September 11, 2018 6:00 PM**

Meeting Minutes

CALL TO ORDER

ROLL CALL: Ken Sipsma, Dean Grosskopf, Eileen Bruskewitz, Brad Zeman, Brian Malich, Tim Black

MINUTES

1 August 14, 2018

Motion by Dean Grosskopf to To Approve , seconded by Brian Malich. Motion carried unanimously:

Ayes: Eileen Bruskewitz, Dean Grosskopf, Ken Sipsma, Brian Malich, Chris Thomas

Not Present: Tim Black, Pat McGowan

Motion Pass. For: 5 - Eileen Bruskewitz, Dean Grosskopf, Ken Sipsma, Brian Malich, Chris Thomas, Against: 0, Absent: 2.

PUBLIC COMMENT

NEW BUSINESS

1 Initial Consultation, Mini Storage Units, Spahn, 5490 CTH Q, Town of Westport

Mike Spahn presented plan and responded to questions. Members were not favorable to a rezone, but directed applicant to work with staff on possible alternatives for this site. Consultation only. No formal action taken.

2 Initial Consultation, Dremsa, Proposed Additional Residence on Lot located at 5846 Endres Road, Town of Westport

Applicant was not present for the meeting. Mr. Wilson informed the Joint Plan Commission members that the Town Plan Commission had not shown support for the proposal the previous evening. No action required.

3 Public Hearing and Discussion Action on A request to Amend the GDP for Kilkenny Farms Commons to Consolidate 82 Multi-Family Units Previously Approved for Lots 263/264 to Lot 2 of CSM 14357

Motion by Dean Grosskopf to To Approve Recommend approval of the Kilkenny Farms Commons GDP Amendment, as recommended by the Village Plan Commission, to the Village Board., seconded by Brian Malich. Motion carried unanimously:

Ayes: Dean Grosskopf, Ken Sipsma, Brian Malich, Chris Thomas
Not Present: Eileen Bruskewitz, Tim Black, Pat McGowan

Motion Pass. For: 4 - Dean Grosskopf, Ken Sipsma, Brian Malich, Chris Thomas, Against: 0,
Absent: 3.

4 Woodland Drive/Peaceful Valley Parkway Public Informational Meeting Update

Staff gave an update on the upcoming meeting. No action necessary.

ADJOURN

Respectfully Submitted,

**VILLAGE OF WAUNAKEE/TOWN OF WESTPORT
JOINT PLAN COMMISSION MEETING
Town of Westport Community Room
5387 Mary Lake Road, Town of Westport
December 11, 2018 6:00 PM**

Meeting Minutes

CALL TO ORDER

The meeting was called to order at 6:01 p.m.

ROLL CALL:

Present: Ken Sipsma, Dean Grosskopf, Eileen Bruskewitz, Brad Zeman, Brian Malich, Chris Thomas

Absent: Tim Black

Also Present: Tom Wilson, Kevin Even, Bryan Kleinmaier, Ed Freer, Don Tierney, Jerry Tierney, Mike Lawton, Paul Kardatzke, Derrek Kinzel

MINUTES

1 September 11, 2018

Motion Bruskewitz, second Grosskopf, to defer the minutes to the next meeting to confirm that they are correct. Motion carried.

PUBLIC COMMENT: None

NEW BUSINESS

1 Discuss and Take Action on Certified Survey Map, Site Plan and Specific Implementation Plan for Boston Pizza, Kilkenny Farms Commons

Mr. Even presented the plans and gave an update on the Plan Commission from the previous evening. Motion Grosskopf, second Zeman, to approve the CSM as recommended by the Village Plan Commission. Motion carried. Motion Bruskewitz, second Zeman, to approve the site plan and SIP as recommended by the Village Plan Commission. Motion carried.

2 Initial Consultation, Kilkenny Farms West General Development Plan and Certified Survey Map

Don and Jerry Tierney presented a conceptual proposal for a grocery store and separate convenience store within the proposed CSM. No action necessary.

3 Discuss and Take Action on 4-Lot CSM, Kilarney Way, Kilkenny Farms

Motion Zeman, second Bruskewitz, to recommend approval of the CSM to the Village and Town Boards. Motion carried.

ADJOURN

Motion Grosskopf, second Malich, to adjourn the meeting at 6:38 p.m.

Respectfully Submitted:

Tracy Meinholz, Joint Plan Commission Secretary



WAUNAKEE/WESTPORT JOINT PLAN COMMISSION SUMMARY SHEET

MEETING DATE: 02/12/2019

ITEM: New Business No. 1

TITLE: Public Hearing and Discussion/Action on a Request by Drumlin Ridge Farm Vineyards LLC to Amend the Existing Conditional Use Permit for 6000 River Road

ISSUE SUMMARY: The original CUP was approved in 2015. They are now seeking expanded hours, larger group gatherings and other minor clarifications to the existing permit. This has been noticed as a public hearing and the neighbors have been notified of the meeting.

STAFF RECOMMENDATION:

RECOMMENDED MOTION:

ATTACHMENT(S): Application for amendment and original CUP documents.



DRUMLIN RIDGE FARM VINEYARDS
6000 River Road

DISCLAIMER: The Village of Waunakee does not guarantee the accuracy of the material contained here in and is not responsible for any misuse or misrepresentation of this information or its derivatives.



SCALE: 1" = 244'

VILLAGE OF WAUNAKEE
500 W. Main St
Waunakee, WI 53597
(608) 850-8500

Print Date: 2/5/2019

Drumlin Ridge Farm Vineyards LLC

6000 River Road
Waunakee, WI 53597
January 11, 2019

Village of Waunakee
Planning/Zoning Department
500 West Main Street
Waunakee, WI 53597

Town of Westport
5387 Mary Lake Road
Waunakee, WI 53597

Re: Application for Amendment of
Conditional Use Permit Restriction
Agreement recorded with the Dane
County Register of Deeds on September
14, 2015 as Document Number 5283715
(the "CUP")

Ladies and Gentlemen;

Drumlin Ridge Farm Vineyards LLC, owner of the real estate identified in the above-referenced CUP, hereby respectfully submits its Application for Amendment to the CUP. As you know, the Drumlin Ridge Winery has been operating at 6000 River Road for well over a year now and has enjoyed success from the beginning. Over the time we've been operating, we've learned a great deal about our business and our place in the community. As such, we have found that our customers, primarily members of the Waunakee – Westport municipalities, happily want to see more of us! And, fortunately for us, we want to see more of them. The enclosed Application for Amendment to CUP articulates certain changes to our CUP which will allow the flexibility to give our community just what they have consistently asked of us. We are confident that these requested changes comply with Wisconsin law and the Waunakee ordinance for conditional uses, but also further our goals of remaining a successful winery and agribusiness of the highest standards right here in the Town of Westport.

Of primary importance to our clientele, many of whom are our neighbors along River Road and the subdivisions thereon, we are requesting more flexibility in our hours of operation and occupancy limits to better serve our community. Our building was designed for occupancy in excess of the limit set forth in our original CUP. Of course, we are naturally limited by the restriction for parking on River Road. In addition, our occupancy is limited by the fire code for our building and the size of our parking area. The Town fire inspector has visited Drumlin Ridge Winery and concluded after measuring our space that we have capacity for 100 guests inside our building and within the enclosure on our patio, though we did not receive a written report to include with our current Application for Amendment to the CUP. We've enclosed a site plan of Drumlin Ridge Winery which demonstrates that our parking and overflow parking (overflow parking is not currently paved) exceeds the applicable ordinance relating to the legal occupancy of our structure. We are confident that we can accommodate the maximum number of guests our

building would allow without any burden on parking or the neighborhood surrounding Drumlin Ridge Winery. Further, we believe that more flexibility in our hours, changing our restriction to be consistent with applicable Wisconsin Statutory restriction for wineries, will allow better service for our clientele without any additional burden to our neighbors. Our history proves this. Our original restrictions were based upon speculation and were intended to assure our neighbors that Drumlin Ridge Winery could not operate as a bar. Drumlin Ridge Winery has proven that its mission is to provide our community with a working vineyard, winery and tasting room of the highest standards. Drumlin Ridge Winery is not a bar. Allowing flexibility in these ways allows Drumlin Ridge Winery to further advance our mission and to better serve our community.

Our enclosed proposed Application and Amendment to CUP also includes other requested changes to our CUP intended to clarify language relating to entertainment, marketing, outdoor lighting and construction compliance. We don't anticipate any of these changes to raise any concerns nor do they conflict in any material way with the original intent of the CUP. However, during our operations over the past year, we have found some of the original CUP language confusing to interpret; and these changes are being requested to clarify the language for purposes of both compliance by Drumlin Ridge Winery and enforcement by the Village and the Town.

Enclosed, please find the following documentation and information:

1. Our Conditional Use Permit Application.
2. A copy of our original CUP.
3. A copy of our proposed Amendment to CUP.
4. A copy of our site plan showing our parking layout and overflow parking location, prepared by Quam Engineering, LLC, 4604 Siggelkow Road, Suite A, McFarland, WI 53558.
5. The application fee of \$350.00.
6. The list of names and addresses of neighbors within 100 feet of the Drumlin Ridge Winery property are attached as Exhibit A. We invited all of the neighbors on the attached list, and several others to join us for a special neighborhood open house during which we discussed our requested changes. We feel that generally our neighbors are in support of our requests. Attached on Exhibit B includes a list of the questions or concerns raised at our open house by three or four of the neighboring couples and some ideas regarding how we can address.
7. This letter shall serve as our statement that we believe our proposed Amendment to CUP, if adopted as drafted, complies with Section 133-936 of the Village of Waunakee Ordinances and Wisconsin State law.
8. Drumlin Ridge Winery currently has 8 employees.
9. We are not currently requesting to make any changes to our building or parking, though we are would be agreeable to improving our overflow parking. To date, use of our overflow parking has been extremely limited, even during events where we accommodated parking for our maximum number of guests.

Thank you for considering our request. We look forward to answering your questions and addressing your concerns.

Sincerely,

A handwritten signature in blue ink, appearing to be 'DK' or similar, written over a horizontal line.

Dave and Brenda Korb,
Drumlin Ridge Farm Vineyards LLC
Drumlin Ridge Winery LLC

EXHIBIT A

Neighbors within 100 feet of Drumlin Ridge Winery Property

Mark and Mary Ripp
6030 River Road
Waunakee, WI 53597

Doug and Renee Riendl
6036 River Road
Waunakee, WI 53597

Joann Koster
6078 River Road
Waunakee, WI 53597

Herbert and Kay Beck (farmland only, no building)
6053 Templeton Drive (mailing address)
Waunakee, WI 53597

Helen and Mark Pullen
5995 River Road
Waunakee, WI 53597

Hamid and Charlene Khazae
5985 Cherokee Valley Pass
Waunakee, WI 53597

Richard and Patricia Teslaw
5966 Cherokee Valley Pass
Waunakee, WI 53597

David and Brenda Korb
5972 Cherokee Valley Pass
Waunakee, WI 53597

EXHIBIT B

Neighborhood Questions, Concerns and Commentary

Question/Concern Raised by Neighbors	Commentary
Concern that later hours will disrupt residential feel of neighborhood	<p>- Sound does not appear to be of issue; generally the neighbors had no complaints about noise. It was mentioned that music could be heard on occasion. DRW remains committed to complying with the applicable sound ordinance.</p> <p>- Lighting is of concern to the neighbors. DRW rarely uses outdoor lighting during summer hours, therefore we believe this must be an issue during late fall and winter months when the sun sets early. We expect that during those colder months, our neighbors likely spend less time outdoors anyway and are less likely to be bothered by lighting. Also, because business during winter months is slower, DRW anticipates the possibility for adjustments to its hours of business which may actually decrease the number of evenings when parking lot lights will be on or the number of hours during which they will be on. DRW will also look into ways it can reduce lighting in the parking lot, including the possibility of switching each light separately so only the minimum number of outdoor lights necessary would be on in the evenings. Finally, and most importantly, substantial vegetation exists and has been planted which should further mitigate this issue as the vegetation grows.</p> <p>- Concern about parking on River Road was mentioned as an issue. With the exception of one time when overflow parking was too wet to use, DRW guests have never parked on River Road. DRW has installed signage to advise our guests of that restriction and have located orange cones to draw attention to the signage. DRW is amenable to improving its overflow parking areas if necessary, and has used mulch to mitigate the mud in the overflow area making that parking available when necessary. To date, the overflow parking areas are rarely necessary, even during permitted special events when up to 75 guests are permitted.</p> <p>- Concern about traffic was mentioned as an issue. DRW does occasionally have a near full parking lot (which rarely includes parking in the overflow areas), however, the ebb and flow of traffic at DRW does not generate a substantial increase in traffic on River Road at any one particular time or at all. Traffic generated by DRW pales in comparison to traffic generated by Madison Country Day School during the school year, CaPaul's Christmas Tree Farm during November/December and Mills Fleet Farm all year now that that business has opened.</p>
Concern about use of neighbor's driveways to turnaround	<p>- DRW has a substantial and well-lit sign on River Road indicating the entrance to the property, so we were very surprised to hear that this was an issue. It has come to our attention that certain mapping apps use the Korb personal residence as the address for DRW possibly causing some confusion as to where the entrance is for DRW. As DRW has discovered these issues, it has advised the mapping app administrators of the error and the location for DRW has been corrected. DRW is amenable to adding signage to further indicate</p>

Question/Concern Raised by Neighbors	Commentary
	where the entrance to the property is located, however, we are concerned that additional signage could cause more confusion in that guests may turn at the wayfinding sign rather than the entrance sign.
Concern that advertising is intended to generate large events	- DRW does advertise and intends to continue to advertise because we want people to visit us and experience our vineyard and wines. However, virtually all of DRW small event business arises from small neighborhood or community based organizations in the Westport and Waunakee area wishing to use the facility for meetings. Some examples include: bank board meetings, a group of parents of pre-schoolers, small business associations, etc. DRW does not wish to and cannot, host large events due to the parking and occupancy restrictions that apply to the building, which restrictions cannot be modified as they are outside the CUP restrictions.
Questions were raised about the amount of wine produced	- DRW is an agribusiness and boutique winery that makes wine on the premises. All of our production for our locally grown grapes is on site. DRW only sells wine on site and does not distribute to local restaurants or retail outlets. As with all agriculture business, we are dependent upon weather which is beyond our control, DRW does produce and store wine in order to ensure that we will have wine during periods of time when our vineyard production is low.



Village of Waunakee
Planning/Zoning Department
500 W. Main Street
Waunakee, WI 53597
Phone (608) 850-8500
Fax (608) 849-5628

Conditional Use Permit Application

APPLICANT

Drumlin Ridge Farm Vineyards LLC

ADDRESS

6000 River Road

Waunakee WI 53597

CONTACT PHONE #

608 849 9463

EMAIL ADDRESS

info@DrumlinRidgeVineyards.com

PROJECT LOCATION

6000 River Road, Waunakee

ZONING CLASS

Aq1 holding

CONDITIONAL USE PERMIT REQUESTS SUBMITTED TO THE VILLAGE OF WAUNAKEE PLAN COMMISSION SHALL FOLLOW THE PROCEDURES LISTED BELOW.

1. Applicant meets with staff.
2. Applicant submits application **fee of \$ 350** and packet by 12:00 noon four (4) weeks prior to the Plan Commission meeting. Plan Commission meetings are regularly scheduled for the fourth Monday of the month.
3. Staff meets to review the request.
4. It is Village policy that all costs incurred by outside professional staff members will be charged to the applicant (see Village Ordinance 106-51 (c)).
5. Applicant attends Public Hearing at Plan Commission meeting and action is taken.:
 - a. Notice of Public Hearing shall be given by publication of a Class 2 Notice under WI statutes in the official Village newspaper.
 - b. Owners of properties within one hundred (100) feet of affected property shall be notified at least five (5) days prior to the date of Public Hearing.

THE UNDERSIGNED CERTIFIES THAT HE/SHE HAS FAMILIARIZED HIMSELF/HERSELF WITH THE STATE AND LOCAL CODES AND PROCEDURES PERTAINING TO THIS APPLICATION. THE UNDERSIGNED FURTHER HEREBY CERTIFIES THAT THE INFORMATION CONTAINED IN THIS APPLICATION IS TRUE AND CORRECT.

SIGNED: _____

(OWNER OR OWNER'S AUTHORIZED AGENT'S SIGNATURE)

FOR VILLAGE USE ONLY:

DATE RECEIVED: _____

FEE PAID: _____

RECEIPT NUMBER: _____

**CONDITIONAL USE PERMIT RESTRICTION
AGREEMENT**

(Korb Winery, Waunakee ETZ)

WHEREAS, David Korb and Brenda Korb (the "Owner"), are the record title Owner of the property described below and, located in the Town of Westport, Dane County, Wisconsin (hereinafter called the "Property"):

Sec 12, T 08 N, R 09 E, SE ¼ of NW ¼,
Lot 3, CSM 454, Town of Westport, Dane County,
Wisconsin.

WHEREAS, the Owner has petitioned the Town of Westport (the "Town") and the Village of Waunakee (the "Village") for a Rezoning to A1 and a Conditional Use Permit ("CUP") to Allow a Winery on the Property; and,

WHEREAS, the Town and Village have approved of this rezoning and CUP as meeting the standards of Section 106-847, Village Code, subject to the conditions and restrictions as listed below.

NOW, THEREFORE, for good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, this Agreement is entered into among the Owner and the Town, and these parties agree as follows:

The Town and Village approve the CUP as requested by the Owner for the allowance of a Winery by application submitted on or about March 29, 2015, subject to the following restrictions and conditions:

1. The Property is hereby restricted for no further land division.
2. The Property is hereby restricted to provide for the building locations and sizes generally as shown on the site plan attached hereto at Exhibit A, and no additional buildings shall be allowed without prior approval of the Town Board. All buildings located upon the Property and used in the Winery business shall be referred to herein as "Winery Premises Structures."
3. The design, scale, and character of the Winery Premises Structures shall be compatible with the intended use. Winery Premises Structures shall not use an exterior design style typically associated with large industrial facilities. Structures shall be consistent with the design review approved at Exhibit B.
4. The visibility of Winery Premises Structures from residential streets shall be accentuated through the use of landscaping and other screening devices to help with the retention of the character of the area in conformance with the landscape plan at Exhibit C.
5. The height of any Winery Premises Structure shall be limited to 35 feet. The height limit may be increased to 45 feet where a pitched roof of greater than four in 12 (rise to run) is proposed and at least 50 percent of the aggregate of all Winery Premises Structures located upon the Winery Premises is limited to a height of 35 feet or less.
6. Exterior lighting fixtures shall be of a low intensity and shall be fully shielded with full cut off



8 9 0 3 2 7 6
Tx:8695837

**KRISTI CHLEBOWSKI
DANE COUNTY
REGISTER OF DEEDS**

**DOCUMENT #
5183715**

09/14/2015 2:30 PM

Trans. Fee:

Exempt #:

Rec. Fee: 30.00

Pages: 38

Return to:
Thomas G. Wilson
Town Attorney/Administrator
5387 Mary Lake Road
Waunakee, WI 53597

Parcel Identification No's.
0809-123-8460-6

- design and directed downward to ensure that neither the lamp nor the related reflector interior surface is visible from a location off of the Winery Premises in order to prevent spill over onto adjacent lots under separate ownership. Pole lighting fixtures shall be used only for Special Events. Exterior lighting shall not be installed or operated in a manner that would throw light, either reflected or directly, in an upward direction. Lighting shall conform to the lighting plan at Exhibit D.
7. The number, size, location, and design of required parking spaces shall be adequate to accommodate the permitted occupancy of the tasting rooms in conformance with Exhibit A.
 8. The visibility of parking areas associated with the Winery Premises from residential streets shall be accentuated through the use of landscaping and other screening devices as shown at Exhibit C.
 9. A solid waste management plan shall be submitted for review and approval by the Town Administrator.
 10. A liquid waste (process wastewater from the Winery operation) management plan shall be submitted for review and approval by the Town Administrator.
 11. Tasting room(s) shall be clearly incidental, accessory, and subordinate to the primary operation of the Winery as a production facility.
 12. The primary focus of the Tasting Room shall be the marketing and sale of the wine produced on the Winery Premises.
 13. More than one Tasting room is allowed on the Winery Premises; however, the cumulative floor area of all tasting rooms shall not exceed the floor area approved by the Town.
 14. Tasting Room hours of operation shall be limited to between the hours of 10:00 a.m. and 6:00 p.m. Sunday-Thursday; 10:00 a.m. and 7:00 p.m. Friday and Saturday.
 15. Wine tasting, wine consumption and food consumption may occur outside of the Tasting Room if allowed by terms of the alcohol license for the premises, but occupancy shall be limited as noted herein.
 16. Retail products shall only be offered for sale within the Tasting Room.
 17. Retail products may include:
 - a. Wine and fruit products produced by the Winery operator or bottled or grown on the Winery Premises, including, wine produced off-site by fruit grown off site if the production of such wine is under the supervision of the Winery operator.
 - b. Souvenirs and clothing bearing the logo of the Winery as well as wine-related items and other products that reflect or enhance the character of theme of the Winery.
 - c. Pre-packaged food.
 18. All food service and food preparation facilities shall comply with all applicable Public Health Department requirements.
 19. Food served at the Winery Premises may be prepared on-site or off-site.
 20. Except for food served in association with Cooking Classes, Winemaker Meals and Special Events, food served at a Winery Premises shall:
 - a. Be limited to small plates or appetizer-like portions, and
 - b. Not include menu options or meal service such that the Winery Premises functions primarily as a restaurant, café, or coffee shop.
 21. Food preparation facilities shall only support the permitted type of food service and permitted cooking classes, winemaker meals and special events allowed on the Property.
 22. Tours, winemaker meals, cooking classes and special events shall not result in occupancy of the applicable Winery Premises Structures in excess of that which has been approved by the Town or the maximum number of Winery Visitors permitted at any one time upon the Property as a whole, as approved by the Town in connection with any issued occupancy certificate (for occupants within the Winery Premises Structures) or this Conditional Use

- Permit for the Winery Premises as a whole, as applicable.
23. Special events shall include special invited tours, winemaker meals, cooking classes and other special events, which shall be clearly secondary, subordinate and incidental to the primary agricultural uses of the Winery Premises. Only 12 Special Events shall be allowed within each calendar year, and only upon prior 2 weeks written notice to the Town Administrator which shall include a description of the Special Event. Special Events shall be limited to a total of 75 patrons, shall be ticketed events, and shall take place between the hours of 10:00 a.m. and 10:00 p.m., subject to state licensing requirements.
24. Occupancy for the entire premises shall not exceed 50 patrons at any one time during regular Tasting Room hours, except that 75 patrons for the entire premises shall be allowed for Special Events.
25. Any outdoor music must comply with Village noise levels, only occur during regular business hours or end before 8:00 p.m. for Special Events, and any music must not be audible consistent with those limits.
26. Tents for events shall not be allowed.
27. Parking shall not be allowed for use of the Property on Cherokee Valley Pass or River Road, and the Owner shall take necessary steps to prevent the same and direct users to designated parking areas.
28. The CUP shall be subject to review every 5 years from the date of this document by the Town Board or Waunakee/Westport Joint Planning Committee (the "JPC"), which shall have the authority to consider and require other conditions and restrictions should use of the Property so require additional measures for the protection of the area, in the discretion of the Town Board or JPC. This does not limit the ability of these bodies to undertake any other periodic review and further activity allowed by code or statute, or as may become necessary due to activity in violation of this agreement, the CUP, or other applicable laws.
29. Any buses, trucks, or similar larger vehicles parked for loading or unloading shall not be allowed to idle engines.

The Owner hereby agrees to pay all costs and fees incurred by the Town and Village for any and all reviews, approval requests and document preparation by the Town and Village Attorney or Engineer, or other consultants in relation to the Property.

The terms and provisions hereof shall be binding upon the undersigned, their heirs, successors, and assigns, and shall be recorded in the office of the Register of Deeds for Dane County, Wisconsin to be construed as a covenant running with the land.

This Agreement may be amended or terminated by the mutual agreement of the parties or their respective successors, assigns or heirs.

If this Agreement is terminated, then the Property shall be subject to the restrictions and requirements of the Zoning District by which the lands are designated at the time of termination and the additional restrictions and requirements provided by this Agreement shall be without any further effect.

The restrictions provided for herein shall be enforceable at law or equity against any party who has or acquires an interest in the land subject to the restriction by the following parties who are named as grantees, promisees and beneficiaries with enforcement rights:

- a. The Village Government of the Village of Waunakee, Dane County, Wisconsin, provided that

the lands are under the jurisdiction of a zoning ordinance of said Village at the time the enforcement action is commenced.

- b. The Town Government of the Town of Westport, Dane County, Wisconsin, whether or not the lands are within the governmental jurisdiction of said Town at the time of enforcement action is commenced.
- c. The County Government, Dane County, Wisconsin, provided that the lands are under the zoning jurisdiction of said County at the time the enforcement action is commenced.

13th IN WITNESS THEREOF, the parties below have hereunder set their hands and seals this day of August, 2015.

OWNER

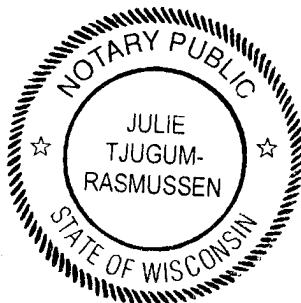
David J. Korb
David J. Korb

Brenda J. Korb
Brenda J. Korb

STATE OF WISCONSIN

COUNTY OF DANE

Personally came before me this 13th day of August, 2015, David J. Korb and Brenda J. Korb, to me known to be the persons who executed the foregoing instrument and acknowledged that they executed the same with property authority.



Julie Tjugum-Rasmussen
Notary Public, State of Wisconsin
My commission expires: 12-14-2018

ICENSSED TO PREFERRED TITLE LLC AND NOT FOR SUBLICENSE, RELICENSE OR ANY OTHER TRANSFER

TOWN OF WESTPORT

By: John A. Van Dinter
John A. Van Dinter
Town Chair

Attest: Thomas G. Wilson
Thomas G. Wilson
Attorney/Administrator/Clerk-Treasurer

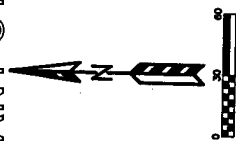
STATE OF WISCONSIN

COUNTY OF DANE

Personally came before me this 17 day of August, 2015, the above-named John A. Van Dinter, Town Chair, and Thomas G. Wilson, Town Attorney/Administrator/Clerk-Treasurer, of the Town of Westport, known to me to be the persons and officers who executed the foregoing instrument and acknowledged that they executed the same as such officers by the Town's authority.

Michelle M. Kumar
Notary Public, State of Wisconsin
My commission expires: 5/1/16

This instrument drafted by:
Thomas G. Wilson
Town Attorney/Administrator
Town of Westport
5387 Mary Lake Road, Waunakee, WI 53597



WORKMAN NOTES:
A STONE CONSTRUCTION ENTRANCE SHALL BE INSTALLED PRIOR TO ANY ENTRANCE. THE STONE CONSTRUCTION ENTRANCE SHALL BE MAINTAINED BY THE CONTRACTOR IN A CONDITION WHICH WILL PREVENT THE TRACK OF SAND OR DIRT SEGMENT ONTO THE ADJACENT PUBLIC STREET. SEGMENT READING THE PUBLIC ROAD SHALL BE REMOVED BY STREET CLEANING (NOT NECESSARILY PAVING) BEFORE THE END OF EACH WORKDAY.

OUT AND THE FOLLOWING SHALL BE NO GREATER THAN 3:1.

THE SCHEDULE

SEPTEMBER 1, 2016
INSTALL RETAIL SHOWROOM CONTROL DEVICES
CONSTRUCT NEW DRIVEWAY, PAVING LOT, AND
THREY AND RESTORE DISTURBED AREAS.
SEPTEMBER 1, 2016 - JULY 31, 2018

REGISTRATION NOTICE

1. FERTILIZER DEFERRED AREAS SHALL RECEIVE A MINIMUM OF FOUR (4) INCHES OF TOPSOIL, AND ALL FERTILIZER DEFERRED AREAS SHALL RECEIVE FERTILIZER EXCEPT NITRAGENITROGEN. NO DEFERRED AREAS SHALL BE USED FOR ANY PURPOSES AS PRACTICAL CONSIDERATIONS WILL BE USED ON ALL DEFERRED AREAS. NITRAGENITROGEN SHALL BE IN EXCESSANCE WITH DEFERRED AREAS. AN EQUAL AMOUNT OF ANNUAL FERTILIZER SHALL BE USED ON ALL DEFERRED AREAS.

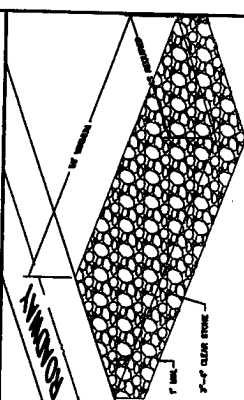
ADDED MATERIALS SHALL BE ADDED TO THE MIX.
CURED MATERIALS SHALL BE APPLIED AT THE RATE OF FOUR (4) POUNDS PER 1,000 SQUARE FEET.
FILLER SHALL BE APPLIED AT THE RATE OF FOUR (4) POUNDS PER 1,000 SQUARE FEET.
EACH SHALL BE COMPOSED OF MAY ON STRAW APPLIED AT THE RATE OF 3 TONS PER ACRE.

[illegible]

UNIQUE, IN 62867
4504, SOCIETY ROAD, SUITE A
MCFARLAND, WY 83055

—●— = DRAINAGE ARROW

NOTES:
1) EXISTING SITE INFORMATION, AERIAL PHOTO, AND CONTOURS OBTAINED FROM THE ACCESS/DANE WEBSITE.



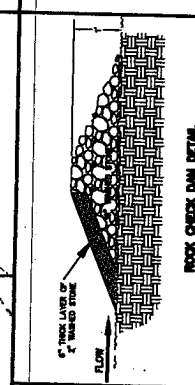
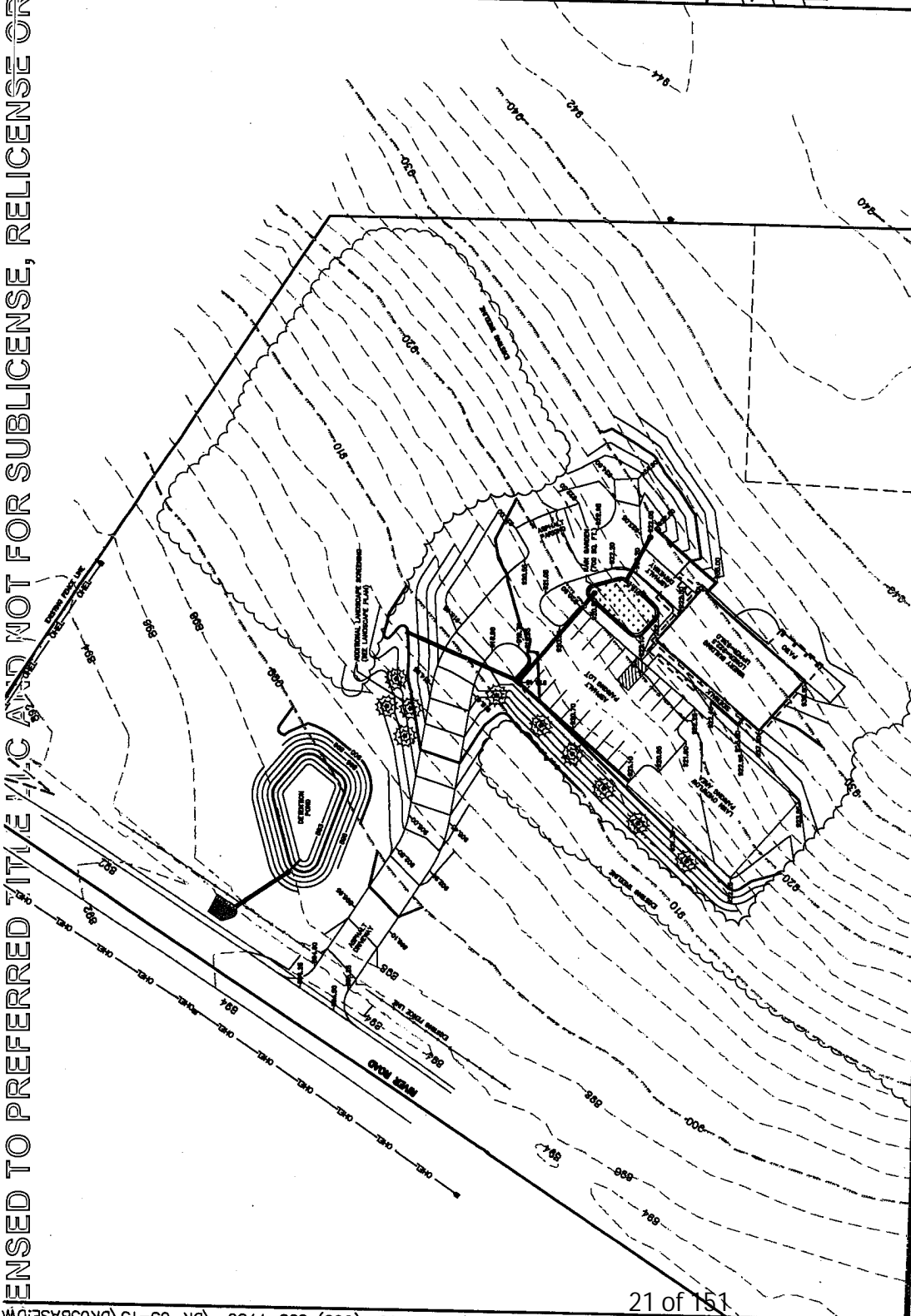
STONE CONSTRUCTION ENTRANCE
 DRUMLIN RIDGE FARM VINEYARDS - WINERY
 BLDG - TOWN OF WESTPORT
 PRELIMINARY GRADING & EROSION CONTROL PLAN
 EXHIBIT #2
 DATED: JULY 8, 2015

QUAM ENGINEERING, LLC

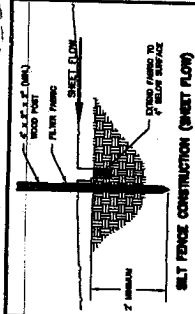


Residential and Commercial Site Design Consultants

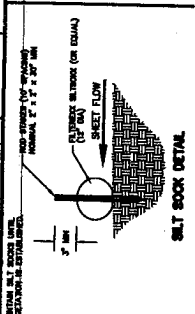
504 Sigwalt Road, Suite A - McFarland, Wisconsin 53558
Phone (808) 838-7750 Fax (808) 838-7751
www.quamengineering.com



BOOK CHECK DAN BETA!



SILT FENCE CONSTRUCTION (SUCTION FLOW)



BUILT SOCK DETAIL

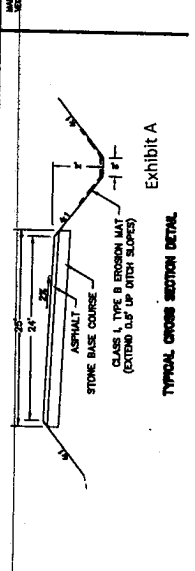


Exhibit A

TYPICAL CROSS SECTION DETAIL

**TOWN OF WESTPORT
DESIGN REVIEW APPLICATION
GENERAL INFORMATION**

Project: Winery

Name: Drumlin Ridge Farm Vineyards, LLC (Winery)

Address: per submitted plans north of Cherokee
Valley Pass on the east side of
River Road

Applicant:

Name: Brenda & David Korb

Address: 5972 Cherokee Valley Pass
Waunakee, WI 53597

Telephone Number: 608-251-6870

Representative:

Name: Kirk Keller

Address: 2310 Crossroads Dr.
Madison, WI

Telephone Number: 608-240-9900 x353

Date Submitted: May 15, 2015

Review Period Ends: June 30, 2015

Period Extended To:

Date of Plan Commission meetings: 6/8+9/15 (PC+JPC)

Time of meeting: 7+6 AM

7/13+14 per agreement w/owner at meetings

I agree that the following information is true to the best of my knowledge, and to abide by Town of Westport Design Review Ordinance Provisions.

By: 

Date: May 15, 2015

Exhibit B

TOWN OF WESTPORT
DESIGN REVIEW APPLICATION
APPROVAL/REJECTION FORM

Date of Approval: 7/13 + 14/15 (after partial approval on 6/8 + 9/15)

Date of Rejection: _____

Reason for Rejection: _____

Approval Conditions: _____
Per minutes of Plan Commission and JPC attached, and plans
per CUP restriction agreement approved.
Revise lights on larger shed on Korb parcel to conform
to Town Code, similar to wine building

Signed:
TOWN OF WESTPORT
By: Shane M. Wilson 7/22/15
Town Administrator

TOWN OF WESTPORT

PLAN COMMISSION B June 8, 2015

The regular monthly meeting of the Plan Commission was called to order at 7:01 p.m. in the Community Meeting Room of the Bernard J. Kennedy Administration Building by Chair Van Dinter. Members present: Grosskopf, Manering, Ruskin, Trotter, and Van Dinter. Members absent: Bowen, Robinson. Also attending: Terry Enge, Bill von Rutenberg, Philip Croom, Manuel Mendoza, Todd Suchomel, Bob Somermeya, Bill Mazanet, Pat and Dick Teslaw, Brenda and David Korb, Rich Trotta, Bill White, Jo Ann Koster, Mary and Mark Ripp, James Hames, Hannah Korb, David Korb, Roger Endres, Keenan Korb, Dennis Petzke, Mary Binkley, Kirk Keller, Ruth Borg, Bill White, Gerald Marsden, Dennis Barman, and Tom Wilson.

There was no Public Comment on Matters not on the Agenda. The minutes of the April 13, 2015 regular meeting were approved as presented on a motion by Trotter, second Ruskin.

Enge reported on work of the Park Committee. Wilson reported on the work of the Historic Preservation Committee in Bowen=s absence.

After a brief presentation by Wilson and a review of revised plans in conformance with suggestions by the JPC, the sign Design Review, Waunakee Baptist Church Sign, 5757 Emerald Grove Lane, was approved as presented at the meeting (no clock) on a motion by Grosskopf, second Trotter.

After a brief presentation by Wilson and the owner, and after questions of the members were answered, the Design Review, Marine Warehouse Building, Mazanet Marina, North Blue Bill Park Drive (North of Summit Credit Union), was approved as presented at the meeting and consistent with suggestions by the JPC, with the addition of the pine or arbor vitae screening as presented, with no outside storage allowed once the building is utilized, and with staff to make sure no electricity to the building is appropriate through the building permit process, on a motion by Trotter, second Grosskopf.

After a brief presentation by Wilson and the owner=s representative, and after questions by the members, the Design Review, Office Addition, Skipper Bud=s, 5381 Westport Road, was approved as presented conditioned on Wilson and Bowen reviewing and approving landscaping and modifications to the front of the structure as discussed to break up the solid front, with staff and owner to address parking issues on the paved shoulder, and with any lighting added for the exit to comply with Town dark skies code, on a motion by Grosskopf, second Manering.

For the Rezone and CUP (County A-1Ex to A-1 and Winery), Korb/Drumlin Ridge Farm Vineyards LLC, 5972 Cherokee Valley Pass/River Road (Waunakee ETZ), matter (Including CUP in A-1 for A Winery@), after a brief presentation by Wilson, a presentation by the applicant and consultants, hearing comments from those in favor and opposed, and after a lengthy discussion on the items, the Plan Commission took the following actions on a motion by

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0728151152

B3

Manering, second Trotter:

As to the zoning code revision to add A Winery as a CUP in the A-1 zone, recommended approval;

As to the Rezone and CUP, recommended approval in general as presented with conditions and restrictions as recommended by Wilson in his staff agenda memo, including 7 more feet of ROW dedicated along the property for River Road if necessary for a 40 foot 2 right of way, and subject to review and approval of the final CUP document by the Plan Commission; and,

As to the design review, approved the design review in concept generally with further and final approval to be given at a later Plan Commission meeting including the signs, lighting, and landscaping, conditioned on the approval of the rezone and CUP.

The applicant gave his approval for a 45 day extension on the design review action to accomplish these items.

For Waunakee/Westport Joint Planning Committee Report/Items for Action, after a brief presentation by Wilson and the Church representative, the Rezone and CSM (Church Use and Lot Line Adjustment), First Presbyterian Church, 5763 CTH Q (Waunakee JPA), was recommended for approval with the newly added Church property to be subject to the same restrictions as the current Church property, on a motion by Grosskopf, second Manering. Wilson reported on other items coming before the JPC.

Wilson and Van Dinter reported on items before the Middleton/Westport Joint Zoning Committee.

For Miscellaneous Matters or Forthcoming Events raised, Trotter presented a report on signage issues in the Town Center and the Chair said he would discuss the same with the Village President.

Motion to adjourn by Grosskopf. Second Ruskin. The meeting adjourned at 8:50 p.m.

Mary Manering, Secretary

TOWN OF WESTPORT

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B4



Village of Waunakee and Town of Westport
Joint Plan Commission

A regular meeting of the Waunakee/Westport Joint Plan Commission was held
Tuesday, July 14, 2015, 6:00 p.m. at the Town of Westport Community Room,
5387 Mary Lake Road, Town of Westport.

CALL TO ORDER

Mr. McGowan called the meeting to order at 6:08 p.m.

Members Present: Ken Sipsma, Dean Grosskopf, John Van Dinter, Mark McGuire, Pat McGowan,
Brad Zeman

Members Absent: None

Also Present: Tom Wilson, Kevin Even, Tracy Meinholz, Bryan Kleinmaier, Joe Capaul, Bill
White, Jim Lundberg, Mark & Mary Ripp, Kay Beck, Jo Ann Koster, David Korb, Hannah
Korb, Keenan Korb, Monica Paffenroth, Ed Niebuhr, Pat Teslaw, Tom Clark, Roger Endres Sr.,
Rich Trotta, Arnold & Jean Bye, Roberta Baumann, Brad Robinson, Kirk Keller.

APPROVAL OF MINUTES

Motion Zeman, second Grosskopf, to approve the minutes from the June 9, 2015 Joint Plan
Commission meeting. Motion carried.

PUBLIC COMMENT

None

NEW BUSINESS

**Discussion / Action on Conditional Use Permit and Design Review for Drumlin Ridge
Farm Vineyards LLC, Town of Westport**

Mr. Sipsma arrived at 6:27 p.m.

After an update from Wilson, a presentation by the owners' consultant, and discussion of
approvals required, motion Sipsma, second Zeman to confirm approval of the "winery"
CUP for the property by adopting the resolution drafted by the Village Attorney with added
language that it be conditioned upon Village Board approval of rezone and the terms of the
presented CUP agreement as revised subject to approval by the Town Board, approve the
restriction agreement as presented by Wilson with additions as discussed and subject to
revisions and approval by the Town Board, and approve the design review/site plan review
as presented and consistent with the July 13 Town Plan Commission action (sign sizes to be
confirmed appropriate by Even), with Wilson directed to review the line fence between the



Korb and Ripp parcels for compliance. Motion carried 5-1 with McGowan opposed.

Discussion / Action on Capaul Request for Detachment and Rezone of Approximately 43.3 acres from A-1 Agricultural/Holding to A1-E Exclusive Agricultural

After discussion, motion Sipsma, second McGuire to recommend approval of the detachment and rezone to the Village and Town Boards subject to the deed restriction document on future annexation as presented by the Village Attorney with alternate language providing for JPC review prior to annexation. Motion carried.

Discussion / Action on Amendment to Environmental Corridors, Waunakee School Site, Woodland Drive West of CTH Q, Town of Westport

Motion Sipsma, second Van Dinter, to approve the requested revision to the environmental corridors for the Waunakee school site on Woodland Drive. Motion carried.

ADJOURN

Motion McGuire, second Van Dinter, to adjourn the meeting at 7:16 p.m. Motion carried.

Submitted By: Tracy Meinholz, Recording Secretary

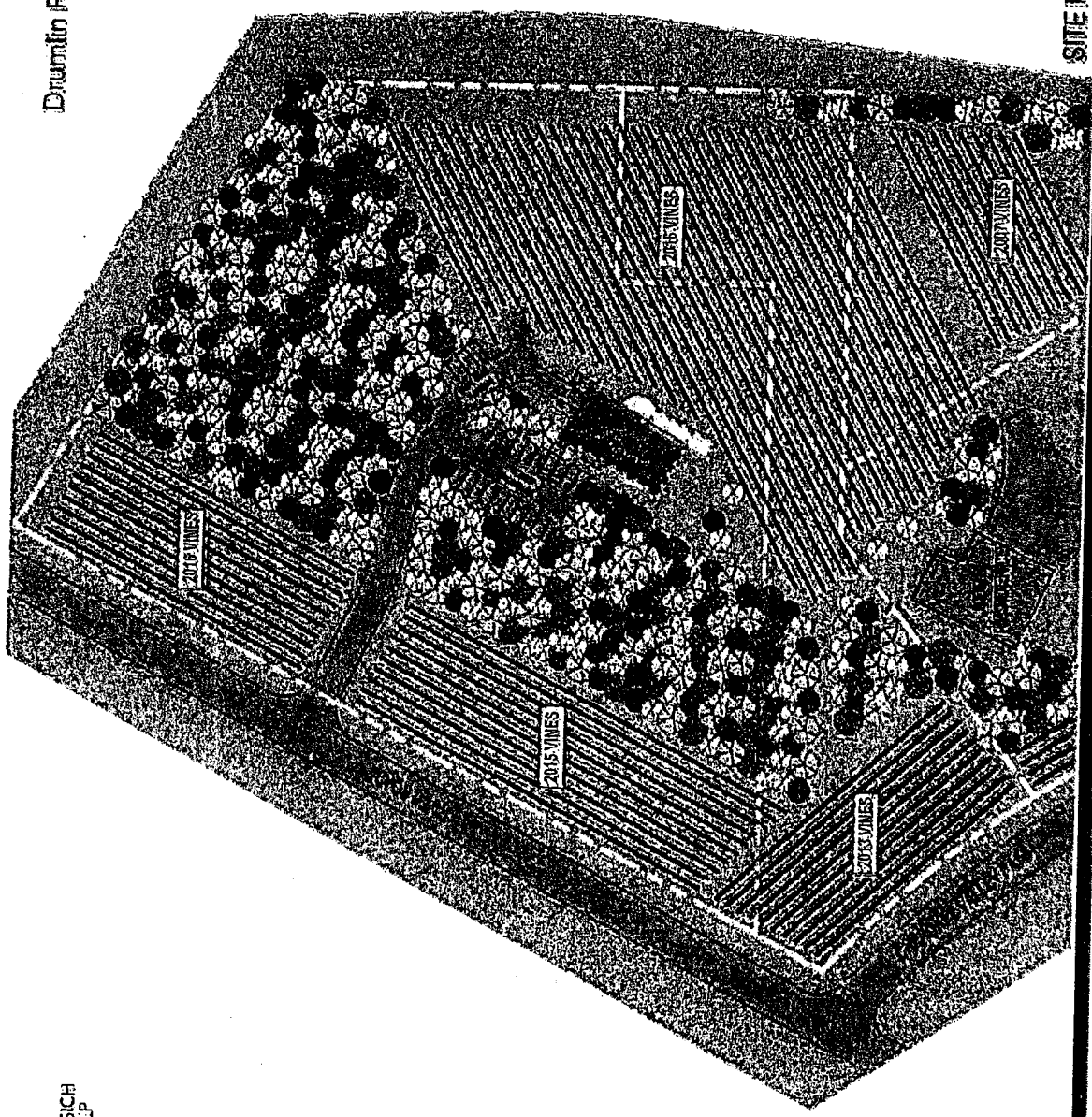
Approved: _____



pran

PLUNKETT-RANSICH
ARCHITECTS, LLP

Drumlin Ridge Farm Vineyards, LLC
Westport, WI



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05-14-15

Scale: 1" = 80'

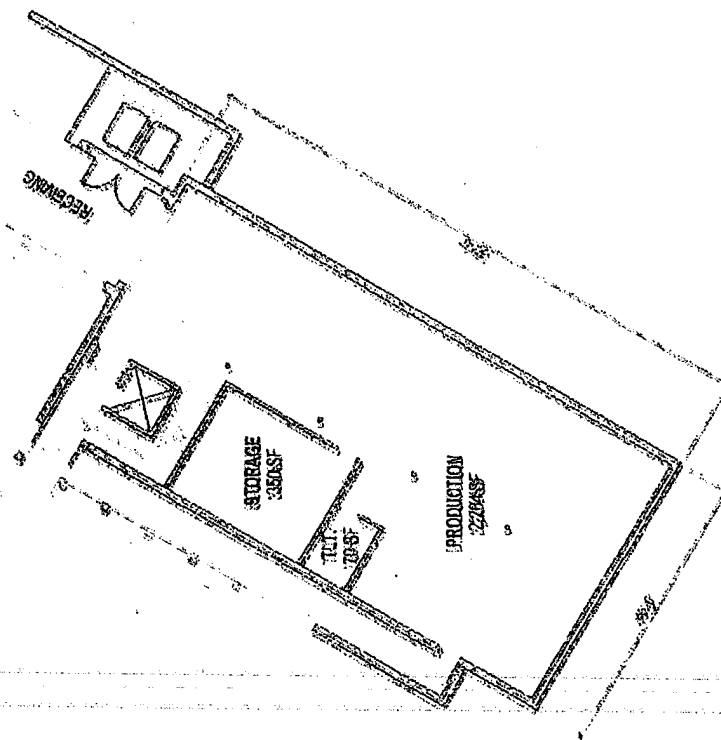
SITE PLAN

ITEM #1.

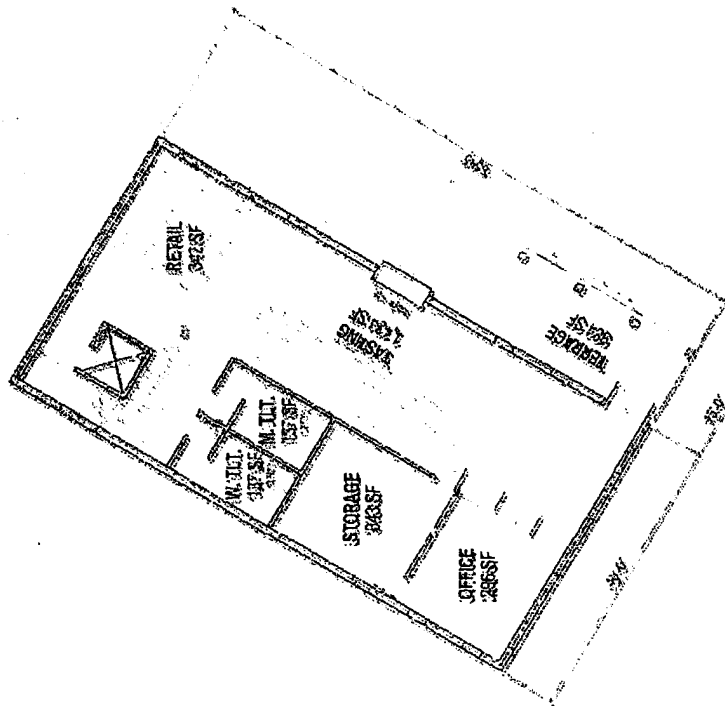
ITEM #1.

Drumlin Ridge Farm Vineyards, LLC
Westport, WI

pran
PRUNKENBRASCH
ARCHITECTS, LLP



GROUND LEVEL FLOOR PLAN
3,200 GSF



MAIN LEVEL FLOOR PLAN
3,200 GSF



1500065-01

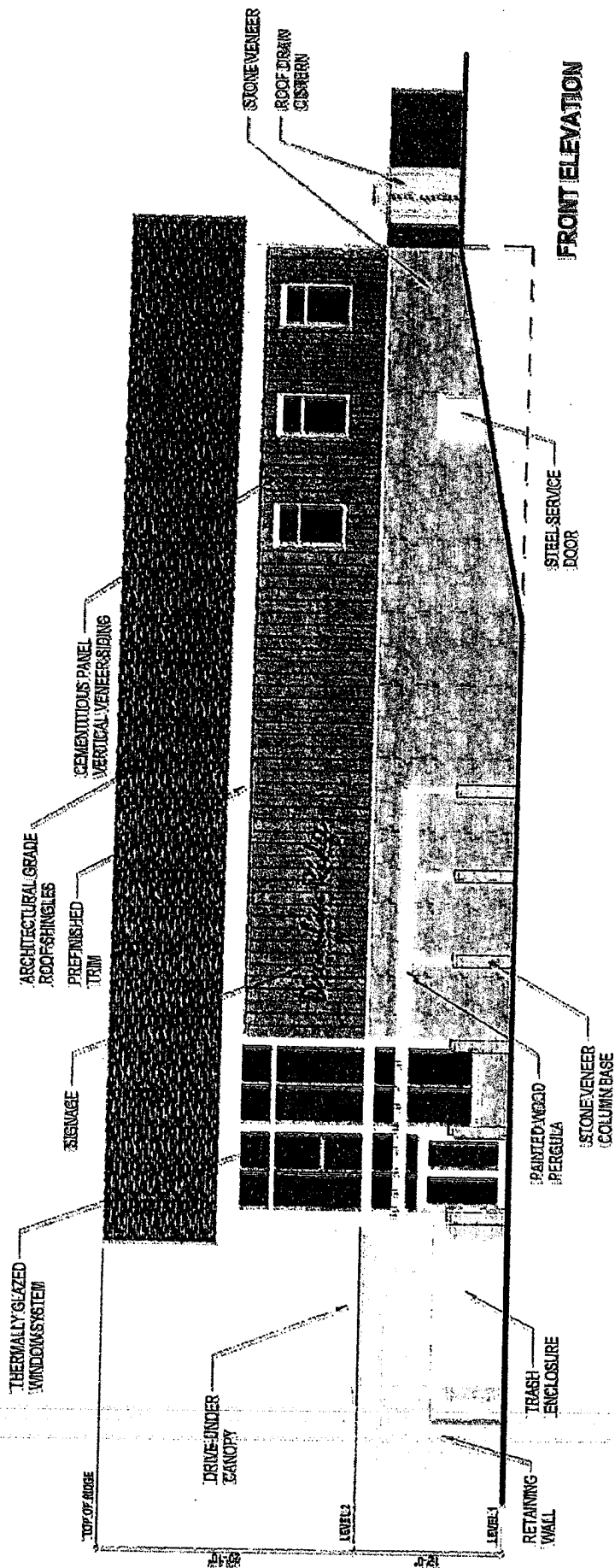
05-14-15

Scale: 1/8" = 1'-0"

pra

PLUNKETT RAYSICH
ARCHITECTS, LLP

Drumlin Ridge Farm Vineyards, LLC
Westport, WI



Scale: 1/8" = 1'-0"

150035-01

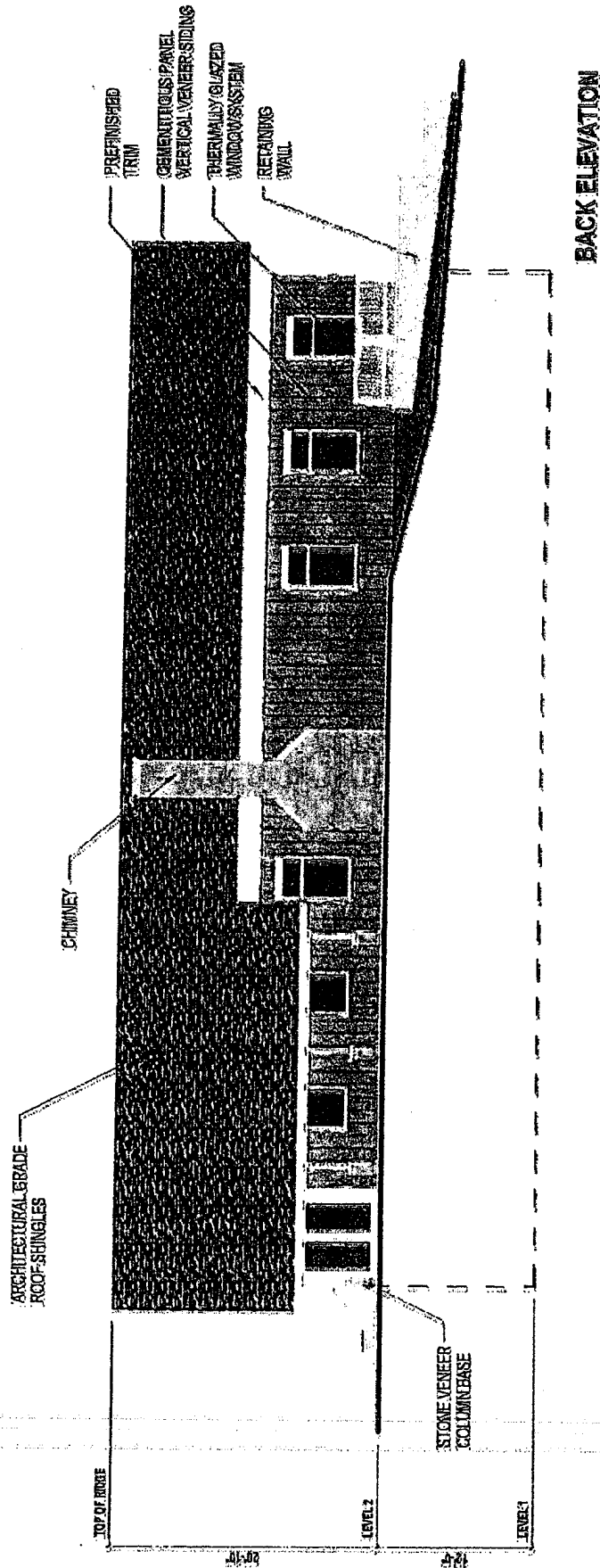
05-14-15

ITEM #1.

prad

PERKINS, RANSICH
ARCHITECTS, LLP

Drumlin Ridge Farm Vineyards, LLC
Westport, WI



Scale: 1/8" = 1'-0"

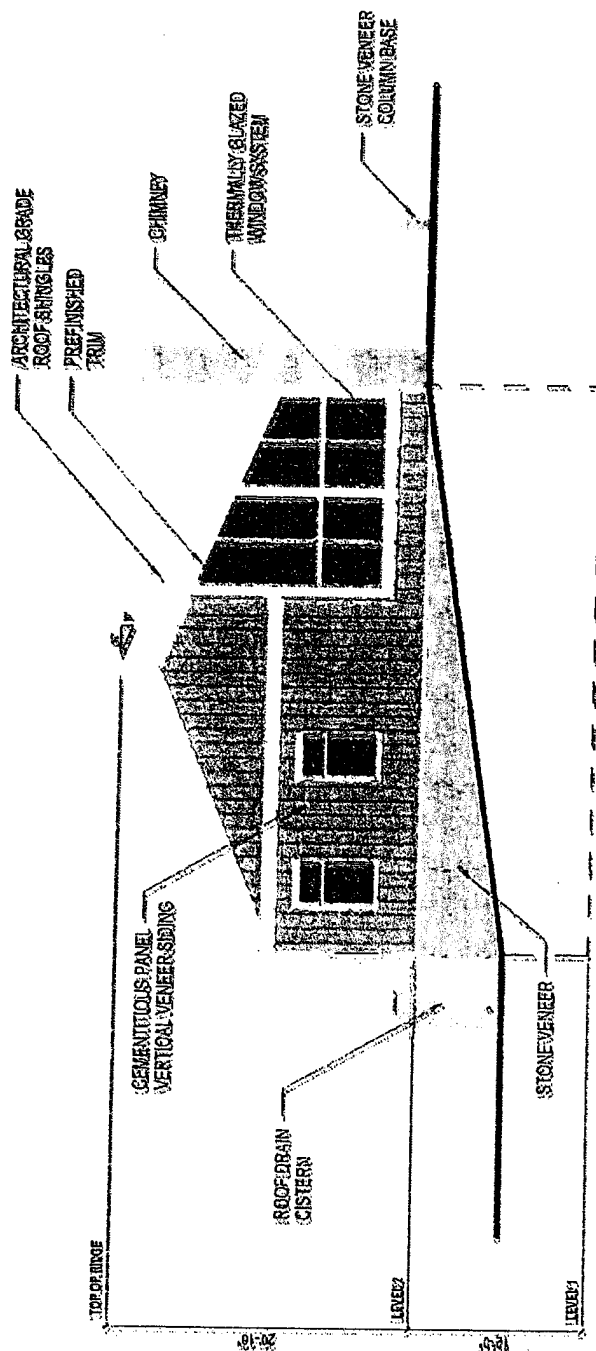
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PHILIPKEIT RANSCH
ARCHITECTS, LLP

Drumlin Ridge Farm Vineyards, LLC
Westport, WI



Scale: 1/8" = 1'-0"

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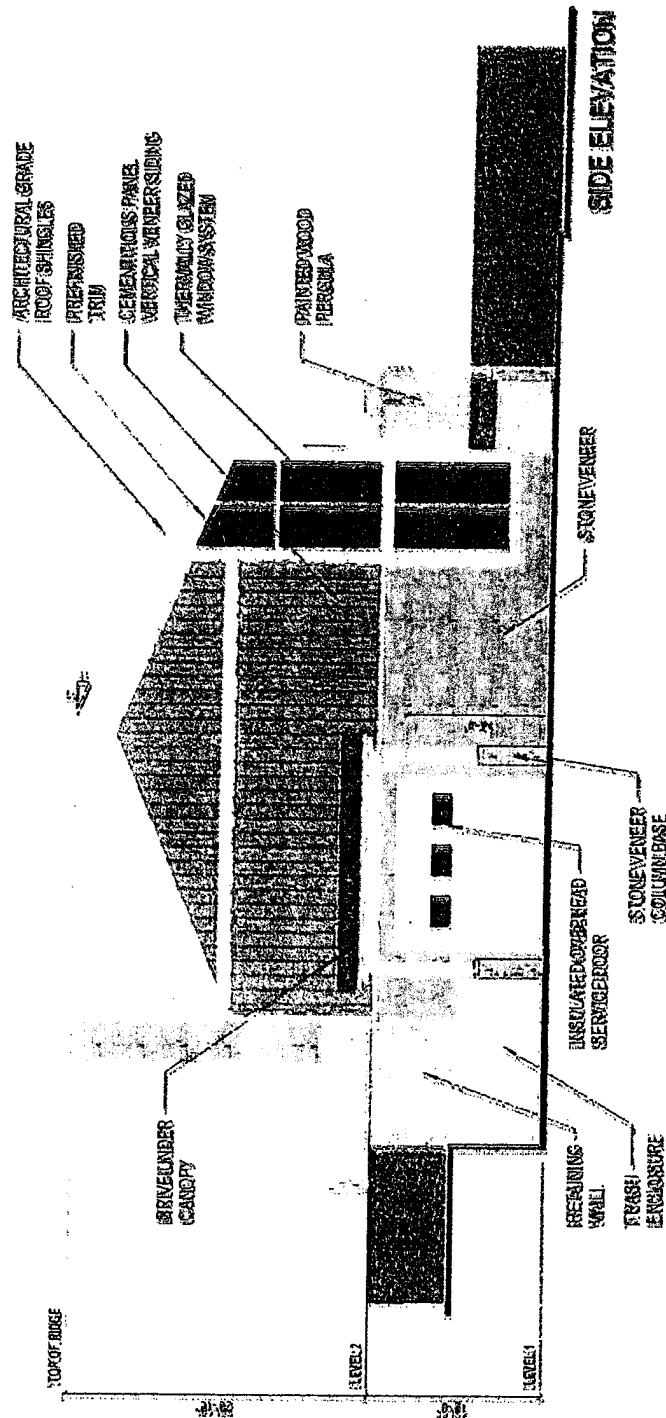
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ITEM #1.

Drumlin Ridge Farm Vineyards, LLC
Westport, WI

pratt

PHILIP KETT BAREIGH
ARCHITECTS, LLP



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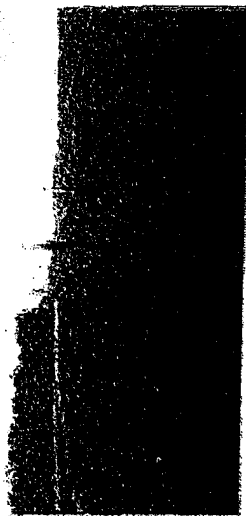
prad

PLENICKET RAYSON
ARCHITECTS, LLP

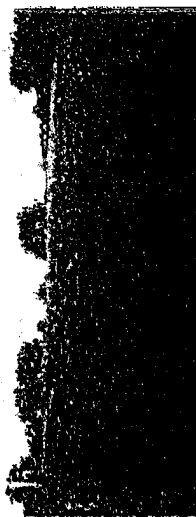
Drumlin Ridge Farm Vineyards, LLC
Westport, WI



View along River Road - Future vineyard area



View to northeast along property line



View from River Road at north property line



View to northwest and approximately point to site

ITEM #1.

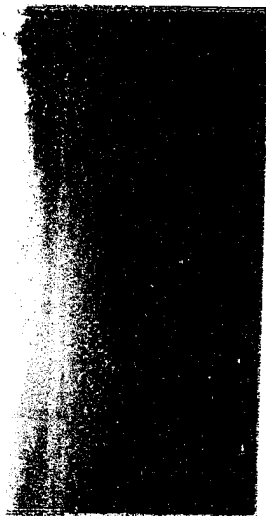
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PUNKETTRAVSICH
ARCHITECTS, LLP

Drumlin Ridge Farm Vineyards, LLC
Westport, WI



View south and approximate vineyard location



View west and approximate vineyard location



North property line with existing pine tree screening

LICENSED TO PREFERRED TITLE LLC AND NOT FOR SUBLICENSE, RELICENSE OR ANY OTHER TRANSFER

HALO LED ICAT HOUSING for NEW CONSTRUCTION

The H750ICAT is a dedicated LED new construction housing to be used with designated HALO LED modules. The H750ICAT is designed for insulated ceilings and can be in direct contact with ceiling insulation*. This AIRTITE housing design prevents airflow between conditioned and unconditioned spaces and saves on both heating and air conditioning costs. The LED connector system provides high efficacy code compliance when used with designated HALO LED modules and trims.

HALO®

Catalog #		Type
Project		
Comments		Date
Prepared by		

DESIGN FEATURES

Housings

Aluminum construction for greater heat dissipation. H750 ICAT housing is gasketed to prevent airflow from heated or air conditioned spaces.

Plaster Frame

Galvanized steel frame. Housing adjusts in plaster frame to accommodate up to 1" ceiling thickness. Regressed locking screw for securing hanger bars. Cutouts included for easily crimping hanger bars in position.

Slide-N-Side™ Junction Box

- Positioned to accommodate straight conduit runs.
- Seven ½" trade size conduit knockouts with true pry-out slots.
- Slide-N-Side wire traps allow non metallic sheathed cable to be installed without tools and without removing knockouts.
- Allows wiring connections to be made outside the box.
- Simply insert the cable directly into the trap after connections are made.
- Accommodates the following standard non-metallic sheathed cable type:
 - U.S. #14/2, #14/3, #12/2, #12/3
 - Canada: #14/2, #14/3, #12/2

GOT NAIL! Pass -N-Thru™ Bar Hangers

Bar Hanger features include

- Pre-installed nail easily installs in regular lumber, engineered lumber and laminated beams.
- Safety and Guidance system prevents snagging, ensures smooth, straight nail penetration and allows bar hangers to be easily removed if necessary.
- Automatic levelling flange aligns the housing and allows holding the housing in place with one hand while driving nails.
- Housing can be positioned at any point within 24" joist spans.
- Score lines allow tool-free shortening for 12" joists and bar hangers do not need to be removed for shortening.
- Bar hangers may be repositioned 90° on plaster frame.
- Integral T-bar clip snaps onto T-bars – no additional clips are required.

LED Module Connection

Halo LED modules simply install with a plug-in 120V-277V rated line voltage wiring connector (UL and CSA Listed Luminaire Disconnect). This non-screw-base connection preserves the high efficacy rating and prevents use of low efficacy incandescent sources (see LED Module specifications).

Caution

Connection is rated for 120V and 277V input. Installer must verify LED module voltage is compatible with the applicable voltage input. If uncertain, consult a qualified electrician.

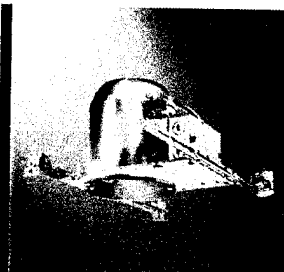
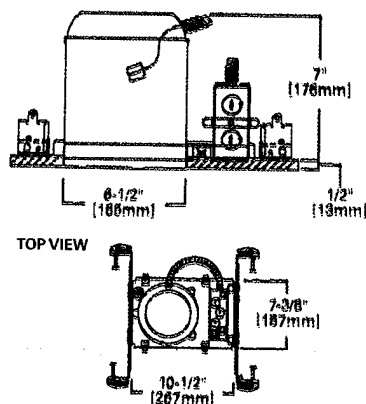
Labels

- UL/cUL Listed 1598 Luminaire
- CE Marking - "Conformité Européenne" conformity with the Council of European Communities Directives, meeting internationally recognized compliance when used with ML5606xx and ML5609xx LED modules
- UL/cUL Listed for Feed Through
- UL/cUL Listed for Damp Location
- UL/cUL Listed for Wet Location with select trims
- UL/cUL Listed for direct contact with insulation and combustible material*
- Rated for 20W maximum

Qualification

May be used with qualified Halo LED modules and designated trims for High Efficacy Luminaire Compliance:

- State of California Title 24
- International Energy Conservation Code (IECC)
- Washington State Energy Code
- New York State Energy Conservation Construction Code - AIR-TITE™ Compliant
- Certified under ASTM-E263 standard for air-tight construction



H750ICAT

6" New Construction
IC AIR-TITE™ Housing
For
Halo LED Modules and
Trims

- ML56 Series
- RL56 Series
- ML7 Series

High Efficacy LED Housing

FOR USE IN
INSULATED CEILINGS

FOR DIRECT CONTACT
WITH INSULATION*

airtite™



Qualified and compliant with select trims. Refer to ENERGY STAR® Qualified Products List and CEC (T24) Appliance Database for listings.

COOPER Lighting
www.cooperlighting.com

* Not to be used in direct contact with spray foam insulation.

LICENSED TO PREFERRED TITLE LLC AND NOT FOR SUBLICENSE, RELICENSE OR ANY OTHER TRANSFER

Juno®

G3.1.1

4" MINIATURE UNIVERSAL TC NEW CONSTRUCTION HOUSING

INCANDESCENT LAMPS

TC1

Project: _____

Fixture Type: _____

Location: _____

Contact/Phone: _____

PRODUCT DESCRIPTION

TC New Construction Housing design for use in non-insulated areas • If installed where insulation is present, the insulation must be pulled back 3" from all sides of the TC housing.

PRODUCT SPECIFICATIONS

Lamp Lamp ratings based on trim selected – see reverse side.

Socket Detachable medium base porcelain with nickel-plated copper screw shell.

Trims Trim selection shown on reverse side.

Labels U.L. listed for through-branch wiring, damp locations and IP • Product thermally protected against improper use of lamps or insulation • Union made • UL Listed/CSA certified • Trim No. 11, 12, 4101, 4102 and 4181 are wet location approved for covered ceiling applications • Trims 14, 17 are wet location approved for covered ceiling applications, **when used with outdoor rated lamps.**

Testing All reports are based on published industry procedures; field performance may differ from laboratory performance.

Product specifications subject to change without notice.

INSTALLATION

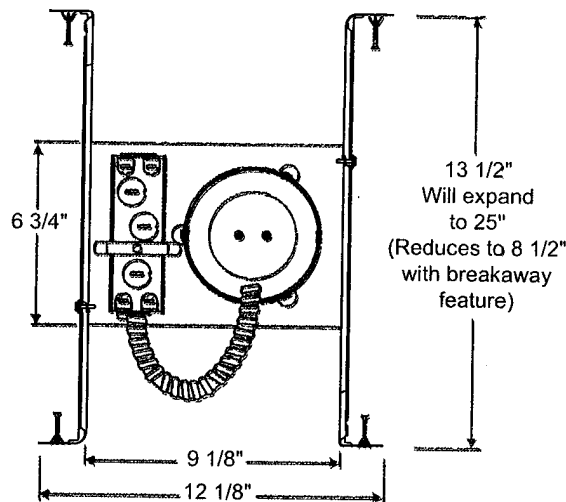
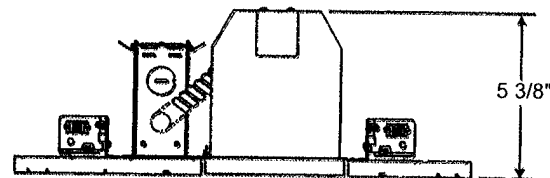
Real Nail 3 Bar Hangers Telescoping Real Nail® 3 system covered under Patent D552,969 permits quick placement of housing anywhere within 24" O.C. joists or suspended ceilings • Integral T-bar notch and clip secures housing in suspended ceiling grid – no accessory clips required • 24" expansion stop allows quick placement of fixture in standard grid spacing • Bars scored in two locations for fast, clean breaking, allowing housing installation in tight applications • Bars captive to mounting frame • Edge-mounted for extra strength • Captive bugle-headed ring shank nail for quick one-step installation and easy removal with claw hammer for fixture relocation • Quick-Loc slot (location identified on the mounting frame) and oversized locking set screw lock fixture in position • Bar hanger foot contoured to align to bottom of construction joist • Alternate mounting holes included.

Junction Box Pre-wired junction box provided with (5) 1/2" and (1) 3/4" knockouts, (4) Non-metallic sheathed cable connectors and ground wire, U.L. listed/CSA certified for through-branch wiring, maximum 8 No. 12 AWG 90° C branch circuit conductors (4 in, 4 out) • Junction box provided with removable access plates • Knockouts equipped with pryout slots.

Mounting Frame 22-gauge die-formed galvanized steel mounting frame • 9 1/8" L x 6 1/4" W, (excluding bar hangers) • Rough-in section (junction box, mounting frame, housing and bar hangers) fully assembled for ease of installation.

Housing TC housing, 22-gauge galvanized steel • Housing is vertically adjustable to accommodate up to a 1 1/4" ceiling thickness.

DIMENSIONS



4 1/2" CEILING CUTOUT

ACCESSORIES

Catalog No.	Description
4TSA	Torsion Spring Adapter (for original style housing)
GU10-WHIP	GU10 Socket Whip

To order, specify catalog number.

PRODUCT CODES

Catalog Number	Input Voltage	Lamp Rating (max.)
TC1	120V	R14/R16/R20/PAR16/PAR20 (See reverse for max. trim rating)

REV-7/14

B17

LICENSED TO PREFERRED TITLE LLC AND NOT FOR SUBLICENSE, RELICENSE OR ANY OTHER TRANSFER

DESCRIPTION

The patented Lumark Crosstour™ LED Wall Pack Series of luminaries provides an architectural style with super bright, energy efficient LEDs. The low-profile, rugged die-cast aluminum construction, universal back box, stainless steel hardware along with a sealed and gasketed optical compartment make the Crosstour impervious to contaminants. The Crosstour wall luminaire is ideal for wall/surface, inverted mount for facade/canopy illumination, post/bollard, site lighting, floodlight and low level pathway illumination including stairs. Typical applications include building entrances, multi-use facilities, apartment buildings, institutions, schools, stairways and loading docks test.

SPECIFICATION FEATURES

Construction

Slim, low-profile LED design with rugged one-piece, die-cast aluminum hinged removable door and back box. Matching housing styles incorporate both a small and large design. The small housing is available in 7W and 18W. The large housing is available in the 26W model. Patent pending secure lock hinge feature allows for safe and easy tool-less electrical connections with the supplied push-in connectors. Back box includes three (3) half-inch, NPT threaded conduit entry points. The universal back box supports both the small and large forms and mounts to standard 3-1/2" to 4" round and octagonal, 4" square, single gang and masonry junction boxes. Key hole gasket allows for adaptation to junction box or wall. **External fin design extracts heat from the fixture surface.** One-piece silicone gasket seals door and back box. Minimum 5" wide pole for site lighting application. Not recommended for car wash applications.

Optical

Silicone sealed optical LED chamber incorporates a custom engineered mirrored anodized reflector providing high-efficiency illumination. Optical assembly includes impact-resistant tempered glass and meets IESNA requirements for full cutoff compliance. Solid state LED Crosstour luminaries are thermally optimized with five (5) lumen packages in cool 5000K or neutral warm 3500K LED color temperature (CCT).

Electrical

LED driver is mounted to the die-cast housing for optimal heat sinking. LED thermal management system incorporates both conduction and natural convection to transfer heat rapidly away from the LED source. 7W models operate in -40°C to 40°C [-40°F to 104°F]. 18W and 26W models operate in -40°C to 40°C [-40°F to 104°F]. High ambient 50°C models available. Crosstour luminaires maintain greater than 90% of initial

light output after 72,000 hours of operation. Three (3) half-inch NPT threaded conduit entry points allow for thru-branch wiring. Back box is an authorized electrical wiring compartment. Integral LED electronic driver incorporates surge protection. 120-277V 50/60Hz or 347V 60Hz models.

Finish

Crosstour is protected with a Super durable TGIC carbon bronze or summit white polyester powder coat paint. Super durable TGIC powder coat paint finishes withstand extreme climate conditions while providing optimal color and gloss retention of the installed life.

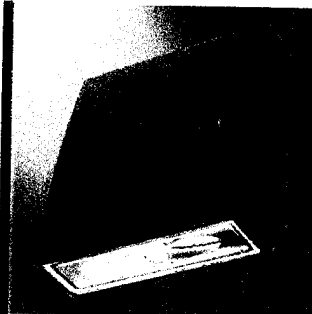
Warranty

Five-year warranty.

LUMARK®



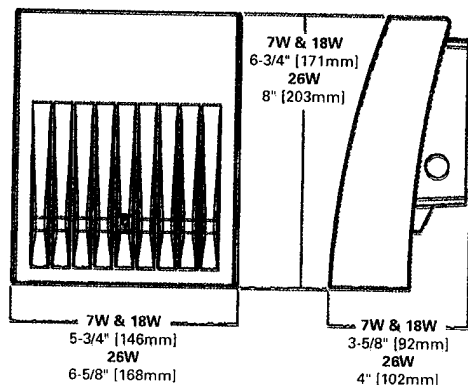
Catalog #		Type
Project		
Comments		Date
Prepared by		



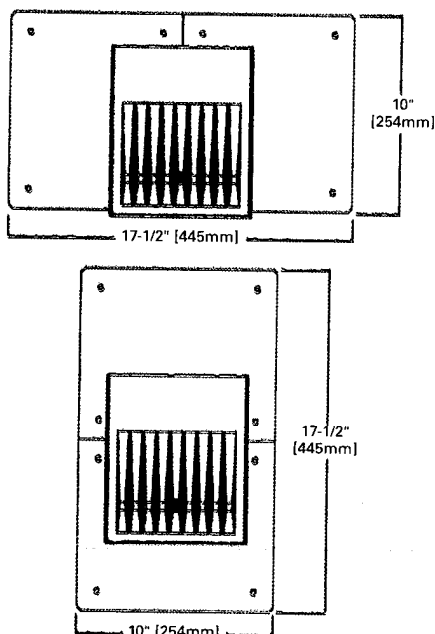
XTOR
CROSSTOUR LED

APPLICATIONS:
WALL / SURFACE
POST / BOLLARD
LOW LEVEL
FLOODLIGHT
INVERTED
SITE LIGHTING

DIMENSIONS



ESCUTCHEON PLATES



CERTIFICATION DATA

UL/cUL Wet Location Listed
LM79 / LM80 Compliant
ROHS Compliant
ADA Compliant
NOM Compliant Models
IP66 Ingress Protection Rated
Title 24 Compliant

TECHNICAL DATA

40°C Maximum Ambient Temperature
External Supply Wiring 90°C Minimum

EPA

Effective Projected Area:
(Sq. Ft.)
XTOR1A/XTOR2A=0.34
XTOR3A = 0.45

SHIPPING DATA:

Approximate Net Weight:
3.7 – 5.25 lbs. [1.7 – 2.4 kgs.]

Cooper Lighting
by E.T.N

B18

DESCRIPTION

The Galleon™ LED luminaire delivers exceptional performance in a highly scalable, low-profile design. Patented, high-efficiency AccuLED Optics™ system provides uniform and energy conscious illumination to walkways, parking lots, roadways, building areas and security lighting applications. IP66 rated.

SPECIFICATION FEATURES

Construction

Extruded aluminum driver enclosure thermally isolated from Light Squares for optimal thermal performance. Heavy-wall, die-cast aluminum end caps enclose housing and die-cast aluminum heat sinks. A unique, patent pending interlocking housing and heat sink provides scalability with superior structural rigidity. 3G vibration tested. Optional tool-less hardware available for ease of entry into electrical chamber. Housing is IP66 rated.

Optics

Choice of 16 patented, high-efficiency AccuLED Optics. The optics are precisely designed to shape the distribution maximizing efficiency and application spacing. AccuLED Optics create consistent distributions with the scalability to meet customized application requirements. Offered standard in 4000K (+/- 275K) CCT and minimum 70 CRI. Optional 6000K CCT and 3000K CCT. For the ultimate level of spill light control, an optional house side shield accessory can be field or factory installed. The

house side shield is designed to seamlessly integrate with the SL2, SL3, SL4 or AFL optics.

Electrical

LED drivers are mounted to removable tray assembly for ease of maintenance. 120-277V 50/60Hz, 347V 60Hz or 480V 60Hz operation. 480V is compatible for use with 480V Wye systems only. Standard with 0-10V dimming. Shipped standard with Eaton proprietary circuit module designed to withstand 10kV of transient line surge. The Galleon LED luminaire is suitable for operation in -40°C to 40°C ambient environments. For applications with ambient temperatures exceeding 40°C, specify the HA (High Ambient) option. Light Squares are IP66 rated. Greater than 90% lumen maintenance expected at 60,000 hours. Available in standard 1A drive current and optional 530mA and 700mA drive currents.

Mounting

Extruded aluminum arm includes internal bolt guides allowing for easy positioning of fixture during

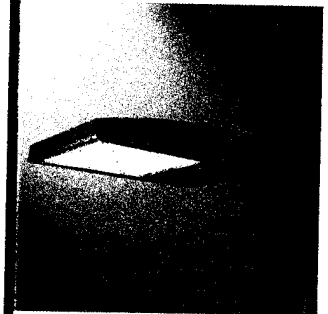
assembly. Designed for pole or wall mounting. When mounting two or more luminaires at 90° or 120° apart, the EA extended arm may be required. Refer to the arm mounting requirement table on page 3. Round pole top adapter included. For wall mounting, specify wall mount bracket option. 3G vibration rated.

Finish

Housing finished in super durable TGIC polyester powder coat paint, 2.5 mil nominal thickness for superior protection against fade and wear. Heat sink is powder coated black. Standard colors include black, bronze, grey, white, dark platinum and graphite metallic. RAL and custom color matches available. Consult the McGraw-Edison Architectural Colors brochure for the complete selection.

Warranty

Five-year warranty.



GLEON
GALLEON LED

1-10 Light Squares
Solid State LED

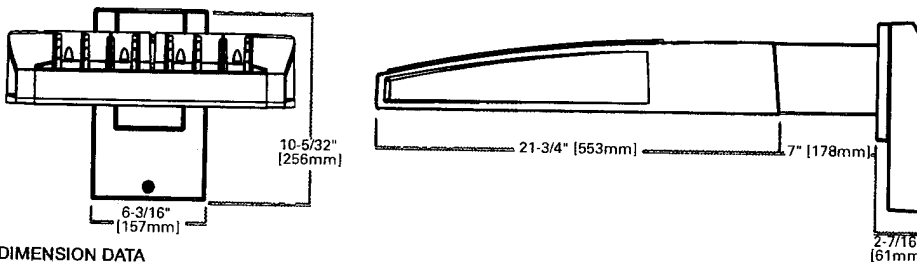
AREA/SITE LUMINAIRE

DIMENSIONS

POLE MOUNT



WALL MOUNT



DIMENSION DATA

Number of Light Squares	"A" Width	"B" Standard Arm Length	"B" Optional Arm Length 1	Weight with Arm (lbs.)	EPA with Arm 2 (Sq. Ft.)
1-4	15-1/2" (394mm)	7" (178mm)	10" (254mm)	33 (15.0 kgs.)	0.96
5-6	21-5/8" (549mm)	7" (178mm)	10" (254mm)	44 (20.0 kgs.)	1.00
7-8	27-5/8" (702mm)	7" (178mm)	13" (330mm)	54 (24.5 kgs.)	1.07
9-10	33-3/4" (857mm)	7" (178mm)	16" (406mm)	63 (28.6 kgs.)	1.12

NOTES: 1 Extended arm option may be required when mounting two or more fixtures per pole at 90° or 120°. Refer to arm mounting requirement table. 2 EPA calculated with optional arm length.



CERTIFICATION DATA

UL/cUL Wet Location Listed
ISO 9001
LM79 / LM80 Compliant
3G Vibration Rated
IP66 Rated
DesignLights Consortium® Qualified*

ENERGY DATA

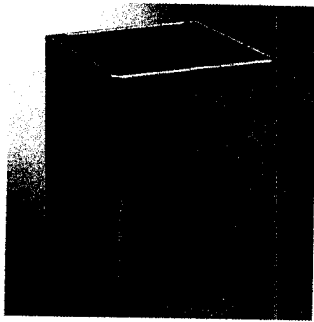
Electronic LED Driver
>0.9 Power Factor
<20% Total Harmonic Distortion
120V-277V 50/60Hz
347V & 480V 60Hz
-40°C Min. Temperature
40°C Max. Temperature
50°C Max. Temperature (HA Option)



TD500020EN
2015-06-04 13:56:33

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COOPER LIGHTING



SSS SQUARE STRAIGHT STEEL

Catalog #		Type
Project		
Comments		Date
Prepared by		

FEATURES

- ASTM Grade steel base plate with ASTM A366 base cover
- Hand hole assembly 3" x 5" on 5" and 6" pole; and 2" x 4" on 4" pole
- 10'-39' mounting heights
- Drilled or tenon (specify)

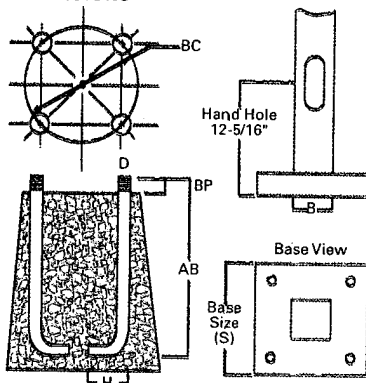
ORDERING INFORMATION

SAMPLE NUMBER: SSS5A20SFM1XG

SSS=Square Straight Steel	4=4" 5=5" 6=6"	A=0.120" M=0.188" X=0.250"	10=10' 15=15' 20=20' 25=25' 30=30' 35=35' 39=39'	S=Square Steel Base	F=Dark Bronze G=Galvanized Steel J=Summit White K=Carbon Bronze L=Dark Platinum P=Primer Powder Coat R=Hartford Green S=Silver T=Graphite Metallic V=Grey W=White X=Custom Color Y=Black	2=2-3/8" O.D. Tenon (4" Long) 3=3-1/2" O.D. Tenon (5" Long) 4=4" O.D. Tenon (6" Long) 5=3" O.D. Tenon (4" Long) 6=2-3/8" O.D. Tenon (6" Long) 7=4" O.D. Tenon (10" Long) A=Type A Drilling C=Type C Drilling E=Type E Drilling F=Type F Drilling G=Type G Drilling J=Type J Drilling K=Type K Drilling M=Type M Drilling R=Type R Drilling Z=Type Z Drilling	1=Single 2=2 at 180° 3=Triple ² 4=4 at 90° 5=2 at 90° X=None	X=None	A=1/2" Tapped Hub (Specify location desired) B=3/4" Tapped Hub (Specify location desired) C=Convenience Outlet ³ E=GFCI Convenience Outlet ³ G=Ground Lug H=Additional Hand Hole ⁴ L=Drilled for Bumper Glitter V=Vibration Dampener
---------------------------------	----------------------	----------------------------------	--	---------------------------	--	---	--	--------	--

NOTES: 1. All shaft sizes nominal. 2. Square poles are 3 at 90°, round poles are 3 at 120°. 3. Outlet is located 4" above base and on same side of pole as hand hole, unless specified otherwise. Receptacle not included, provision only. 4. Additional hand hole is located 12" below pole top and 90° from standard hand hole location, unless otherwise specified.

DIMENSIONS



WARNING: The use of unauthorized accessories such as banners, signs, cameras or pennants for which the pole was not designed voids the pole warranty from Eaton's Cooper Lighting business and may result in pole failure causing serious injury or property damage. Upon request, Eaton's Cooper Lighting business will supply information regarding total loading capacity. The pole warranty from Eaton's Cooper Lighting business is void unless poles are used and installed as a complete pole/luminaire combination. This warranty specifically excludes failure as the result of a third party act or omission, misuse, unanticipated uses, fatigue failure or similar phenomena resulting from induced vibration, harmonic oscillation or resonance associated with movement of air currents around the product.

Specifications and dimensions subject to change without notice. Consult your Eaton's Cooper Lighting business representative or visit www.cooperlighting.com for available options, accessories and ordering information.

Cooper Lighting
by EATON

B20

**CONDITIONAL USE PERMIT RESTRICTION
AGREEMENT**

(Korb Winery, Waunakee ETZ)

WHEREAS, David Korb and Brenda Korb (the "Owner"), are the record title Owner of the property described below and, located in the Town of Westport, Dane County, Wisconsin (hereinafter called the "Property"):

Sec 12, T 08 N, R 09 E, SE ¼ of NW ¼,
Lot 3, CSM 454, Town of Westport, Dane County,
Wisconsin.

WHEREAS, the Owner has petitioned the Town of Westport (the "Town") and the Village of Waunakee (the "Village") for a Rezoning to A1 and a Conditional Use Permit ("CUP") to Allow a Winery on the Property; and,

WHEREAS, the Town and Village have approved of this rezoning and CUP as meeting the standards of Section 106-847, Village Code, subject to the conditions and restrictions as listed below.

NOW, THEREFORE, for good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, this Agreement is entered into among the Owner and the Town, and these parties agree as follows:

The Town and Village approve the CUP as requested by the Owner for the allowance of a Winery by application submitted on or about March 29, 2015, subject to the following restrictions and conditions:

1. The Property is hereby restricted for no further land division.
2. The Property is hereby restricted to provide for the building locations and sizes generally as shown on the site plan attached hereto at Exhibit A, and no additional buildings shall be allowed without prior approval of the Town Board. All buildings located upon the Property and used in the Winery business shall be referred to herein as "Winery Premises Structures."
3. The design, scale, and character of the Winery Premises Structures shall be compatible with the intended use. Winery Premises Structures shall not use an exterior design style typically associated with large industrial facilities. Structures shall be consistent with the design review approved at Exhibit B.
4. The visibility of Winery Premises Structures from residential streets shall be accentuated through the use of landscaping and other screening devices to help with the retention of the character of the area in conformance with the landscape plan at Exhibit C.
5. The height of any Winery Premises Structure shall be limited to 35 feet. The height limit may be increased to 45 feet where a pitched roof of greater than four in 12 (rise to run) is proposed and at least 50 percent of the aggregate of all Winery Premises Structures located upon the Winery Premises is limited to a height of 35 feet or less.
6. Exterior lighting fixtures shall be of a low intensity and shall be fully shielded with full cut off

Return to:
Thomas G. Wilson
Town Attorney/Administrator
5387 Mary Lake Road
Waunakee, WI 53597

Parcel Identification No's.
0809-123-8460-6

- design and directed downward to ensure that neither the lamp nor the related reflector interior surface is visible from a location off of the Winery Premises in order to prevent spill over onto adjacent lots under separate ownership. Pole lighting fixtures shall be used only for Special Events. Exterior lighting shall not be installed or operated in a manner that would throw light, either reflected or directly, in an upward direction. Lighting shall conform to the lighting plan at Exhibit D.
7. The number, size, location, and design of required parking spaces shall be adequate to accommodate the permitted occupancy of the tasting rooms in conformance with Exhibit A.
 8. The visibility of parking areas associated with the Winery Premises from residential streets shall be accentuated through the use of landscaping and other screening devices as shown at Exhibit C.
 9. A solid waste management plan shall be submitted for review and approval by the Town Administrator.
 10. A liquid waste (process wastewater from the Winery operation) management plan shall be submitted for review and approval by the Town Administrator.
 11. Tasting room(s) shall be clearly incidental, accessory, and subordinate to the primary operation of the Winery as a production facility.
 12. The primary focus of the Tasting Room shall be the marketing and sale of the wine produced on the Winery Premises.
 13. More than one Tasting room is allowed on the Winery Premises; however, the cumulative floor area of all tasting rooms shall not exceed the floor area approved by the Town.
 14. Tasting Room hours of operation shall be limited to between the hours of 10:00 a.m. and 6:00 p.m. Sunday-Thursday; 10:00 a.m. and 7:00 p.m. Friday and Saturday.
 15. Wine tasting, wine consumption and food consumption may occur outside of the Tasting Room if allowed by terms of the alcohol license for the premises, but occupancy shall be limited as noted herein.
 16. Retail products shall only be offered for sale within the Tasting Room.
 17. Retail products may include:
 - a. Wine and fruit products produced by the Winery operator or bottled or grown on the Winery Premises, including, wine produced off-site by fruit grown off site if the production of such wine is under the supervision of the Winery operator.
 - b. Souvenirs and clothing bearing the logo of the Winery as well as wine-related items and other products that reflect or enhance the character of theme of the Winery.
 - c. Pre-packaged food.
 18. All food service and food preparation facilities shall comply with all applicable Public Health Department requirements.
 19. Food served at the Winery Premises may be prepared on-site or off-site.
 20. Except for food served in association with Cooking Classes, Winemaker Meals and Special Events, food served at a Winery Premises shall:
 - a. Be limited to small plates or appetizer-like portions, and
 - b. Not include menu options or meal service such that the Winery Premises functions primarily as a restaurant, café, or coffee shop.
 21. Food preparation facilities shall only support the permitted type of food service and permitted cooking classes, winemaker meals and special events allowed on the Property.
 22. Tours, winemaker meals, cooking classes and special events shall not result in occupancy of the applicable Winery Premises Structures in excess of that which has been approved by the Town or the maximum number of Winery Visitors permitted at any one time upon the Property as a whole, as approved by the Town in connection with any issued occupancy certificate (for occupants within the Winery Premises Structures) or this Conditional Use

- Permit for the Winery Premises as a whole, as applicable.
23. Special events shall include special invited tours, winemaker meals, cooking classes and other special events, which shall be clearly secondary, subordinate and incidental to the primary agricultural uses of the Winery Premises. Only 12 Special Events shall be allowed within each calendar year, and only upon prior 2 weeks written notice to the Town Administrator which shall include a description of the Special Event. Special Events shall be limited to a total of 75 patrons, shall be ticketed events, and shall take place between the hours of 10:00 a.m. and 10:00 p.m., subject to state licensing requirements.
24. Occupancy for the entire premises shall not exceed 50 patrons at any one time during regular Tasting Room hours, except that 75 patrons for the entire premises shall be allowed for Special Events.
25. Any outdoor music must comply with Village noise levels, only occur during regular business hours or end before 8:00 p.m. for Special Events, and any music must not be audible consistent with those limits.
26. Tents for events shall not be allowed.
27. Parking shall not be allowed for use of the Property on Cherokee Valley Pass or River Road, and the Owner shall take necessary steps to prevent the same and direct users to designated parking areas.
28. The CUP shall be subject to review every 5 years from the date of this document by the Town Board or Waunakee/Westport Joint Planning Committee (the "JPC"), which shall have the authority to consider and require other conditions and restrictions should use of the Property so require additional measures for the protection of the area, in the discretion of the Town Board or JPC. This does not limit the ability of these bodies to undertake any other periodic review and further activity allowed by code or statute, or as may become necessary due to activity in violation of this agreement, the CUP, or other applicable laws.
29. Any buses, trucks, or similar larger vehicles parked for loading or unloading shall not be allowed to idle engines.

The Owner hereby agrees to pay all costs and fees incurred by the Town and Village for any and all reviews, approval requests and document preparation by the Town and Village Attorney or Engineer, or other consultants in relation to the Property.

The terms and provisions hereof shall be binding upon the undersigned, their heirs, successors, and assigns, and shall be recorded in the office of the Register of Deeds for Dane County, Wisconsin to be construed as a covenant running with the land.

This Agreement may be amended or terminated by the mutual agreement of the parties or their respective successors, assigns or heirs.

If this Agreement is terminated, then the Property shall be subject to the restrictions and requirements of the Zoning District by which the lands are designated at the time of termination and the additional restrictions and requirements provided by this Agreement shall be without any further effect.

The restrictions provided for herein shall be enforceable at law or equity against any party who has or acquires an interest in the land subject to the restriction by the following parties who are named as grantees, promisees and beneficiaries with enforcement rights:

- a. The Village Government of the Village of Waunakee, Dane County, Wisconsin, provided that

the lands are under the jurisdiction of a zoning ordinance of said Village at the time the enforcement action is commenced.

- b. The Town Government of the Town of Westport, Dane County, Wisconsin, whether or not the lands are within the governmental jurisdiction of said Town at the time of enforcement action is commenced.
- c. The County Government, Dane County, Wisconsin, provided that the lands are under the zoning jurisdiction of said County at the time the enforcement action is commenced.

IN WITNESS THEREOF, the parties below have hereunder set their hands and seals this ____ day of _____, 2015.

OWNER

David J. Korb

Brenda J. Korb

STATE OF WISCONSIN

COUNTY OF DANE

Personally came before me this ____ day of _____, 2015, David J. Korb and Brenda J. Korb, to me known to be the persons who executed the foregoing instrument and acknowledged that they executed the same with property authority.

Notary Public, State of Wisconsin

My commission expires: _____

LICENSED TO PREFERRED TITLE LLC AND NOT FOR SUBLICENSE, RELICENSE OR ANY OTHER TRANSFER

TOWN OF WESTPORT

By: _____
John A. Van Dinter
Town Chair

Attest: _____
Thomas G. Wilson
Attorney/Administrator/Clerk-Treasurer

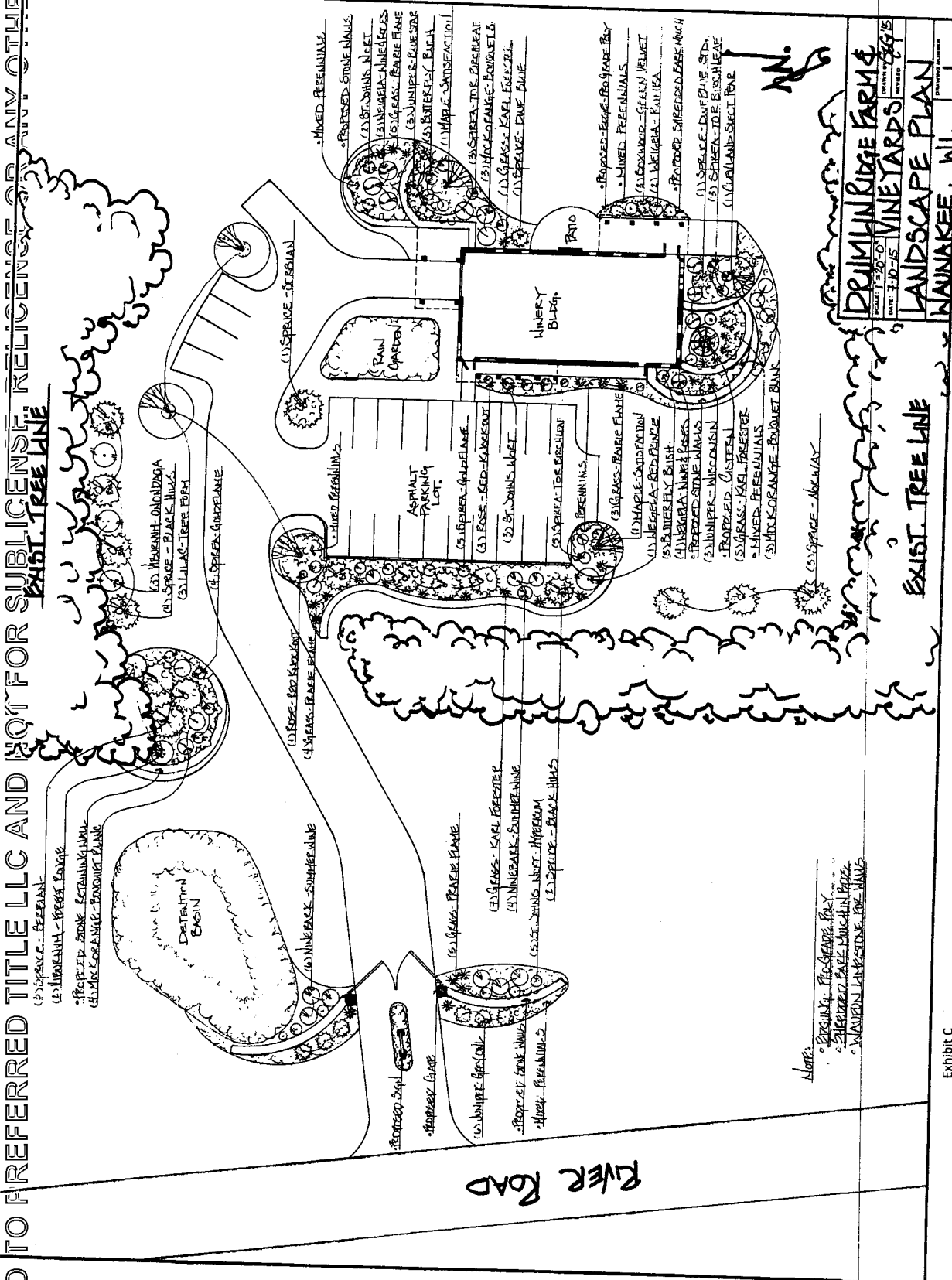
STATE OF WISCONSIN

COUNTY OF DANE

Personally came before me this ____ day of _____, 2015, the above-named John A. Van Dinter, Town Chair, and Thomas G. Wilson, Town Attorney/Administrator/Clerk-Treasurer, of the Town of Westport, known to me to be the persons and officers who executed the foregoing instrument and acknowledged that they executed the same as such officers by the Town's authority.

Notary Public, State of Wisconsin
My commission expires: _____

This instrument drafted by:
Thomas G. Wilson
Town Attorney/Administrator
Town of Westport
5387 Mary Lake Road, Waunakee, WI 53597



LICENSED TO PREFERRED TITLE LLC AND NOT FOR SUBLEASE, RE-LICENSE OR ANY OTHER TRANSFER

WERNER
ALUMINUM SUPPLY
1341 HOUTMAN DRIVE, WESTPORT, MI 48151
(820) 728-4500, (820) 728-4482 FAX
WWW.WERNERALUMSUPPLY.COM

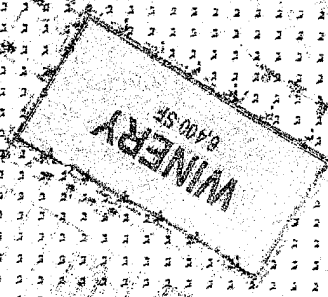
RIVER ROAD WINERY
SITE LIGHTING
WESTPORT, MI

DATE: 11/11/2015
TIME: 11:11 AM
JOB # 00015
PROJECT: RIVER ROAD WINERY
DRAWING: SITE LIGHTING

ES01



EMERGENCY
PARKING



2015 VINES

2015 VINES

RIVER ROAD

Exhibit D

HALO LED ICAT HOUSING for NEW CONSTRUCTION

The H750ICAT is a dedicated LED new construction housing to be used with designated HALO LED modules. The H750ICAT is designed for insulated ceilings and can be in direct contact with ceiling insulation*. This AIRTITE housing design prevents airflow between conditioned and unconditioned spaces and saves on both heating and air conditioning costs. The LED connector system provides high efficacy code compliance when used with designated HALO LED modules and trims.

Catalog #		Type	
Project			
Comments		Date	
Prepared by			

HALO®

DESIGN FEATURES

Housings

Aluminum construction for greater heat dissipation. H750 ICAT housing is gasketed to prevent airflow from heated or air conditioned spaces.

Plaster Frame

Galvanized steel frame. Housing adjusts in plaster frame to accommodate up to 1" ceiling thickness. Regressed locking screw for securing hanger bars. Cutouts included for easily crimping hanger bars in position

Slide-N-Side™ Junction Box

- Positioned to accommodate straight conduit runs.
- Seven ½" trade size conduit knockouts with true pry-out slots.
- Slide-N-Side wire traps allow non metallic sheathed cable to be installed without tools and without removing knockouts.
- Allows wiring connections to be made outside the box.
- Simply insert the cable directly into the trap after connections are made.
- Accommodates the following standard non-metallic sheathed cable type:
- U.S. #14/2, #14/3, #12/2, #12/3
- Canada: #14/2, #14/3, #12/2

GOT NAIL! Pass -N-Thru™ Bar Hangers

Bar Hanger features include

- Pre-installed nail easily installs in regular lumber, engineered lumber and laminated beams.
- Safety and Guidance system prevents snagging, ensures smooth, straight nail penetration and allows bar hangers to be easily removed if necessary
- Automatic leveling flange aligns the housing and allows holding the housing in place with one hand while driving nails.
- Housing can be positioned at any point within 24" joist spans
- Score lines allow tool-free shortening for 12" joists and bar hangers do not need to be removed for shortening.
- Bar hangers may be repositioned 90° on plaster frame
- Integral T-bar clip snaps onto T-bars – no additional clips are required.

LED Module Connection

Halo LED modules simply install with a plug-in 120V-277V rated line voltage wiring connector (UL and CSA Listed Luminaire Disconnect). This non-screw-base connection preserves the high efficacy rating and prevents use of low efficacy incandescent sources (see LED Module specifications).

Caution

Connection is rated for 120V and 277V input. Installer must verify LED module voltage is compatible with the applicable voltage input. If uncertain, consult a qualified electrician.

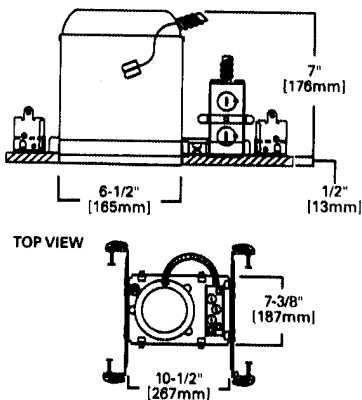
Labels

- UL/cUL Listed 1598 Luminaire
- CE Marking - "Conformité Européene" conformity with the Council of European Communities Directives, meeting internationally recognized compliance when used with ML5606xx and ML5609xx LED modules
- UL/cUL Listed for Feed Through
- UL/cUL Listed for Damp Location
- UL/cUL Listed for Wet Location with select trims
- UL/cUL Listed for direct contact with insulation and combustible material*
- Rated for 20W maximum

Qualification

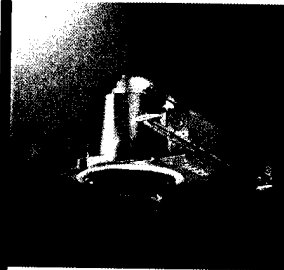
May be used with qualified Halo LED modules and designated trims for High Efficacy Luminaire Compliance:

- State of California Title 24
- International Energy Conservation Code (IECC)
- Washington State Energy Code
- New York State Energy Conservation Construction Code - AIR-TITE™ Compliant
- Certified under ASTM-E283 standard for air-tight construction



COOPER Lighting
www.cooperlighting.com

* Not to be used in direct contact with spray foam insulation.



H750ICAT

6" New Construction
IC AIR-TITE™ Housing
For
Halo LED Modules and
Trims

- ML56 Series
- RL56 Series
- ML7 Series

High Efficacy LED Housing

FOR USE IN
INSULATED CEILINGS

FOR DIRECT CONTACT
WITH INSULATION*

airtite™



Qualified and compliant with select trims. Refer to ENERGY STAR® Qualified Products List and CEC (T24) Appliance Database for listings.

ADV121880
April 8, 2013 9:43 AM

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Juno

G3.1.1

4" MINIATURE UNIVERSAL TC NEW CONSTRUCTION HOUSING

INCANDESCENT LAMPS

TC1

Project: _____

Fixture Type: _____

Location: _____

Contact/Phone: _____

PRODUCT DESCRIPTION

TC New Construction Housing design for use in non-insulated areas • If installed where insulation is present, the insulation must be pulled back 3" from all sides of the TC housing.

PRODUCT SPECIFICATIONS

Lamp Lamp ratings based on trim selected – see reverse side.

Socket Detachable medium base porcelain with nickel-plated copper screw shell.

Trims Trim selection shown on reverse side.

Labels U.L. listed for through-branch wiring, damp locations and IP • Product thermally protected against improper use of lamps or insulation • Union made • UL Listed/CSA certified • Trim No. 11, 12, 4101, 4102 and 4181 are wet location approved for covered ceiling applications • Trims 14, 17 are wet location approved for covered ceiling applications, **when used with outdoor rated lamps.**

Testing All reports are based on published industry procedures; field performance may differ from laboratory performance.

Product specifications subject to change without notice.

INSTALLATION

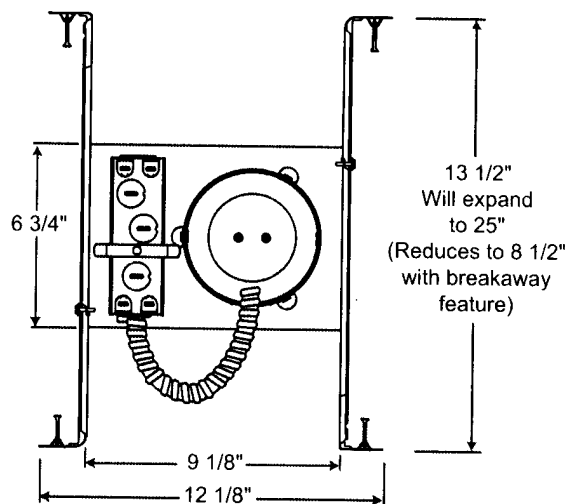
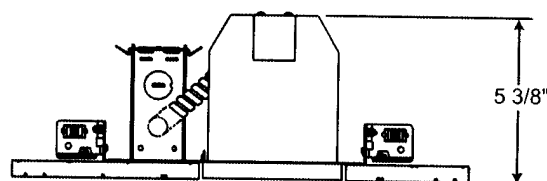
Real Nail 3 Bar Hangers Telescoping Real Nail® 3 system covered under Patent D552,969 permits quick placement of housing anywhere within 24" O.C. joists or suspended ceilings • Integral T-bar notch and clip secures housing in suspended ceiling grid – no accessory clips required • 24" expansion stop allows quick placement of fixture in standard grid spacing • Bars scored in two locations for fast, clean breaking, allowing housing installation in tight applications • Bars captive to mounting frame • Edge-mounted for extra strength • Captive bugle-headed ring shank nail for quick one-step installation and easy removal with claw hammer for fixture relocation • Quick-Loc slot (location identified on the mounting frame) and oversized locking set screw lock fixture in position • Bar hanger foot contoured to align to bottom of construction joist • Alternate mounting holes included.

Junction Box Pre-wired junction box provided with (5) 1/2" and (1) 3/4" knockouts, (4) Non-metallic sheathed cable connectors and ground wire, U.L. listed/CSA certified for through-branch wiring, maximum 8 No. 12 AWG 90° C branch circuit conductors (4 in, 4 out) • Junction box provided with removable access plates • Knockouts equipped with pryout slots.

Mounting Frame 22-gauge die-formed galvanized steel mounting frame • 9 1/8" L x 6 1/8" W, (excluding bar hangers) • Rough-in section (junction box, mounting frame, housing and bar hangers) fully assembled for ease of installation.

Housing TC housing, 22-gauge galvanized steel • Housing is vertically adjustable to accommodate up to a 1 1/4" ceiling thickness.

DIMENSIONS



4 1/2" CEILING CUTOUT

ACCESSORIES

Catalog No.	Description
4TSA	Torsion Spring Adapter (for original style housing)
GU10-WHIP	GU10 Socket Whip

To order, specify catalog number.

PRODUCT CODES

Catalog Number	Input Voltage	Lamp Rating (max.)
TC1	120V	R14/R16/R20/PAR16/PAR20 (See reverse for max. trim rating)

REV-7/14

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DESCRIPTION

The patented Lumark Crosstour™ LED Wall Pack Series of luminaires provides an architectural style with super bright, energy efficient LEDs. The low-profile, rugged die-cast aluminum construction, universal back box, stainless steel hardware along with a sealed and gasketed optical compartment make the Crosstour impervious to contaminants. The Crosstour wall luminaire is ideal for wall/surface, inverted mount for façade/canopy illumination, post/bollard, site lighting, floodlight and low level pathway illumination including stairs. Typical applications include building entrances, multi-use facilities, apartment buildings, institutions, schools, stairways and loading docks test.

SPECIFICATION FEATURES

Construction

Slim, low-profile LED design with rugged one-piece, die-cast aluminum hinged removable door and back box. Matching housing styles incorporate both a small and large design. The small housing is available in 7W and 18W. The large housing is available in the 26W model. Patent pending secure lock hinge feature allows for safe and easy tool-less electrical connections with the supplied push-in connectors. Back box includes three (3) half-inch, NPT threaded conduit entry points. The universal back box supports both the small and large forms and mounts to standard 3-1/2" to 4" round and octagonal, 4" square, single gang and masonry junction boxes. Key hole gasket allows for adaptation to junction box or wall. External fin design extracts heat from the fixture surface. One-piece silicone gasket seals door and back box. Minimum 5" wide pole for site lighting application. Not recommended for car wash applications.

Optical

Silicone sealed optical LED chamber incorporates a custom engineered mirrored anodized reflector providing high-efficiency illumination. Optical assembly includes impact-resistant tempered glass and meets IESNA requirements for full cutoff compliance. Solid state LED Crosstour luminaires are thermally optimized with five (5) lumen packages in cool 5000K or neutral warm 3500K LED color temperature (CCT).

Electrical

LED driver is mounted to the die-cast housing for optimal heat sinking. LED thermal management system incorporates both conduction and natural convection to transfer heat rapidly away from the LED source. 7W models operate in -40°C to 40°C [-40°F to 104°F]. 18W and 26W models operate in -40°C to 40°C [-40°F to 104°F]. High ambient 50°C models available. Crosstour luminaires maintain greater than 90% of initial

light output after 72,000 hours of operation. Three (3) half-inch NPT threaded conduit entry points allow for thru-branch wiring. Back box is an authorized electrical wiring compartment. Integral LED electronic driver incorporates surge protection. 120-277V 50/60Hz or 347V 60Hz models.

Finish

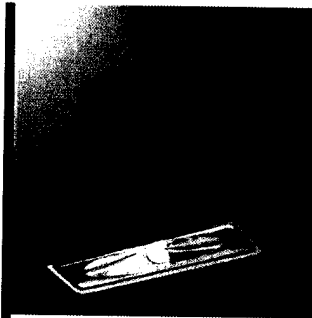
Crosstour is protected with a Super durable TGIC carbon bronze or summit white polyester powder coat paint. Super durable TGIC powder coat paint finishes withstand extreme climate conditions while providing optimal color and gloss retention of the installed life.

Warranty

Five-year warranty.

LUMARK®  energy

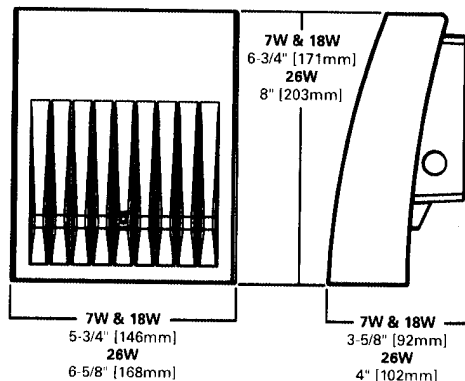
Catalog #		Type
Project		
Comments		Date
Prepared by		



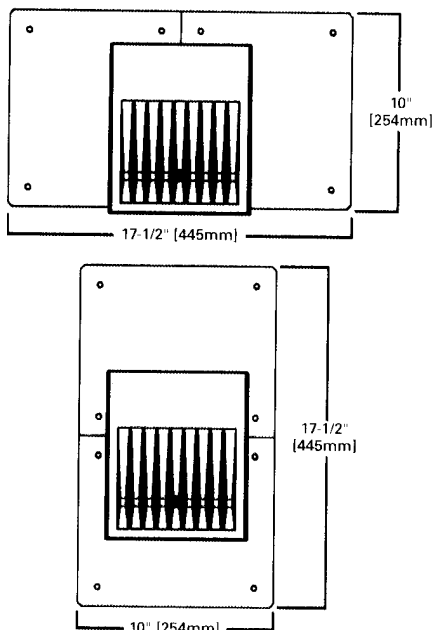
XTOR
CROSSTOUR LED

APPLICATIONS:
WALL / SURFACE
POST / BOLLARD
LOW LEVEL
FLOODLIGHT
INVERTED
SITE LIGHTING

DIMENSIONS



ESCUTCHEON PLATES



CERTIFICATION DATA

UL/cUL Wet Location Listed
LM79 / LM80 Compliant
ROHS Compliant
ADA Compliant
NOM Compliant Models
IP66 Ingress Protection Rated
Title 24 Compliant

TECHNICAL DATA

40°C Maximum Ambient Temperature
External Supply Wiring 90°C Minimum

EPA

Effective Projected Area:
(Sq. Ft.)
XTOR1A/XTOR2A=0.34
XTOR3A = 0.45

SHIPPING DATA:

Approximate Net Weight:
3.7 – 5.25 lbs. [1.7 – 2.4 kgs.]

Cooper Lighting
by E.T.N

DY

LICENSED TO PREFERRED TITLE LLC AND NOT FOR SUBLICENSE, RELICENSE OR ANY OTHER TRANSFER

DESCRIPTION

The Galleon™ LED luminaire delivers exceptional performance in a highly scalable, low-profile design. Patented, high-efficiency AccuLED Optics™ system provides uniform and energy conscious illumination to walkways, parking lots, roadways, building areas and security lighting applications. IP66 rated.

SPECIFICATION FEATURES

Construction

Extruded aluminum driver enclosure thermally isolated from Light Squares for optimal thermal performance. Heavy-wall, die-cast aluminum end caps enclose housing and die-cast aluminum heat sinks. A unique, patent pending interlocking housing and heat sink provides scalability with superior structural rigidity. 3G vibration tested. Optional tool-less hardware available for ease of entry into electrical chamber. Housing is IP66 rated.

Optics

Choice of 16 patented, high-efficiency AccuLED Optics. The optics are precisely designed to shape the distribution maximizing efficiency and application spacing. AccuLED Optics create consistent distributions with the scalability to meet customized application requirements. Offered standard in 4000K (+/- 275K) CCT and minimum 70 CRI. Optional 6000K CCT and 3000K CCT. For the ultimate level of spill light control, an optional house side shield accessory can be field or factory installed. The

house side shield is designed to seamlessly integrate with the SL2, SL3, SL4 or AFL optics.

Electrical

LED drivers are mounted to removable tray assembly for ease of maintenance. 120-277V 50/60Hz, 347V 60Hz or 480V 60Hz operation. 480V is compatible for use with 480V Wye systems only. Standard with 0-10V dimming. Shipped standard with Eaton proprietary circuit module designed to withstand 10kV of transient line surge. The Galleon LED luminaire is suitable for operation in -40°C to 40°C ambient environments. For applications with ambient temperatures exceeding 40°C, specify the HA (High Ambient) option. Light Squares are IP66 rated. Greater than 90% lumen maintenance expected at 60,000 hours. Available in standard 1A drive current and optional 530mA and 700mA drive currents.

Mounting

Extruded aluminum arm includes internal bolt guides allowing for easy positioning of fixture during

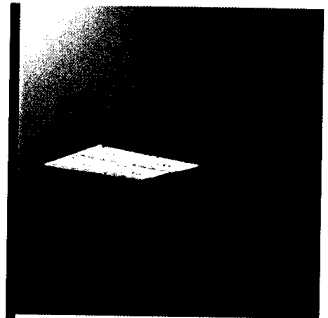
assembly. Designed for pole or wall mounting. When mounting two or more luminaires at 90° or 120° apart, the EA extended arm may be required. Refer to the arm mounting requirement table on page 3. Round pole top adapter included. For wall mounting, specify wall mount bracket option. 3G vibration rated.

Finish

Housing finished in super durable TGIC polyester powder coat paint, 2.5 mil nominal thickness for superior protection against fade and wear. Heat sink is powder coated black. Standard colors include black, bronze, grey, white, dark platinum and graphite metallic. RAL and custom color matches available. Consult the McGraw-Edison Architectural Colors brochure for the complete selection.

Warranty

Five-year warranty.



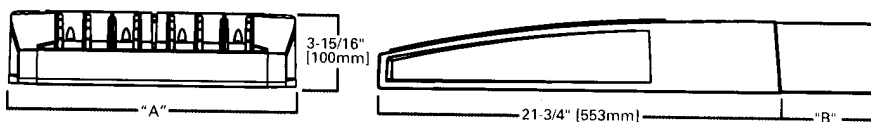
GLEON
GALLEON LED

1-10 Light Squares
Solid State LED

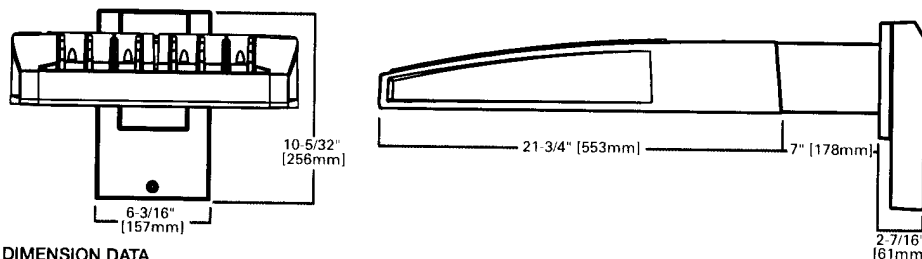
AREA/SITE LUMINAIRE

DIMENSIONS

POLE MOUNT



WALL MOUNT



DIMENSION DATA

Number of Light Squares	"A" Width	"B" Standard Arm Length	"B" Optional Arm Length ¹	Weight with Arm (lbs.)	EPA with Arm ² (Sq. Ft.)
1-4	15-1/2" (394mm)	7" (178mm)	10" (254mm)	33 (15.0 kgs.)	0.96
5-6	21-5/8" (549mm)	7" (178mm)	10" (254mm)	44 (20.0 kgs.)	1.00
7-8	27-5/8" (702mm)	7" (178mm)	13" (330mm)	54 (24.5 kgs.)	1.07
9-10	33-3/4" (857mm)	7" (178mm)	16" (406mm)	63 (28.6 kgs.)	1.12

NOTES: 1 Extended arm option may be required when mounting two or more fixtures per pole at 90° or 120°. Refer to arm mounting requirement table. 2 EPA calculated with optional arm length.



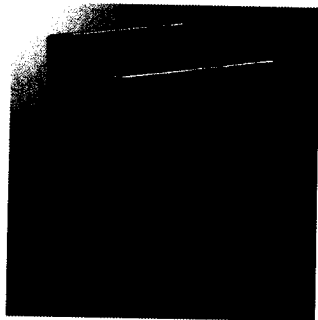
CERTIFICATION DATA

UL/cUL Wet Location Listed
ISO 9001
LM79 / LM80 Compliant
3G Vibration Rated
IP66 Rated
DesignLights Consortium® Qualified*

ENERGY DATA

Electronic LED Driver
>0.9 Power Factor
<20% Total Harmonic Distortion
120V-277V 50/60Hz
347V & 480V 60Hz
-40°C Min. Temperature
40°C Max. Temperature
50°C Max. Temperature (HA Option)

COOPER LIGHTING



SSS SQUARE
STRAIGHT STEEL

Catalog #		Type
Project		
Comments		Date
Prepared by		

- FEATURES
- ASTM Grade steel base plate with ASTM A366 base cover
 - Hand hole assembly 3" x 5" on 5" and 6" pole; and 2" x 4" on 4" pole
 - 10'-39' mounting heights
 - Drilled or tenon (specify)

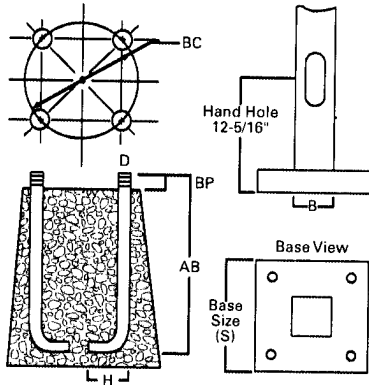
ORDERING INFORMATION

SAMPLE NUMBER: SSS5A20SFM1XG

Product Family	Shaft Size (Inches) ¹	Wall Thickness (Inches)	Mounting Height (Feet)	Base Type	Finish	Mounting Type	Number and Location of Arms	Arm Lengths (Feet)	Options (Add as Suffix)
SSS=Square Straight Steel	4=4"	A=0.120"	10=10'	S=Square Steel Base	F=Dark Bronze	2=2-3/8" O.D. Tenon (4" Long)	1=Single	X=None	A=1/2" Tapped Hub (Specify location desired)
	5=5"	M=0.188"	15=15'		G=Galvanized Steel	3=3-1/2" O.D. Tenon (5" Long)	2=2 at 180°		
	6=6"	X=0.250"	20=20'		J=Summit White	4=4" O.D. Tenon (6" Long)	3=Triple ²		
			25=25'		K=Carbon Bronze	5=3" O.D. Tenon (4" Long)	4=4 at 90°		B=3/4" Tapped Hub (Specify location desired)
			30=30'		L=Dark Platinum	6=2-3/8" O.D. Tenon (6" Long)	5=2 at 90°		C=Convenience Outlet ³
			35=35'		P=Primer Powder Coat	7=4" O.D. Tenon (10" Long)	X=None		E=GFCI Convenience Outlet ³
			39=39'		R=Hartford Green				G=Ground Lug
					S=Silver	A=Type A Drilling			H=Additional Hand Hole ⁴
					T=Graphite Metallic	C=Type C Drilling			L=Drilled for Bumper Glitter
					V=Grey	E=Type E Drilling			V=Vibration Dampener
					W=White	F=Type F Drilling			
					X=Custom Color	G=Type G Drilling			
					Y=Black	J=Type J Drilling			
						K=Type K Drilling			
						M=Type M Drilling			
						R=Type R Drilling			
						Z=Type Z Drilling			

NOTES: 1. All shaft sizes nominal. 2. Square poles are 3 at 90°, round poles are 3 at 120°. 3. Outlet is located 4' above base and on same side of pole as hand hole, unless specified otherwise. Receptacle not included, provision only. 4. Additional hand hole is located 12" below pole top and 90° from standard hand hole location, unless otherwise specified.

DIMENSIONS



WARNING: The use of unauthorized accessories such as banners, signs, cameras or pennants for which the pole was not designed voids the pole warranty from Eaton's Cooper Lighting business and may result in pole failure causing serious injury or property damage. Upon request, Eaton's Cooper Lighting business will supply information regarding total loading capacity. The pole warranty from Eaton's Cooper Lighting business is void unless poles are used and installed as a complete pole/luminaire combination. This warranty specifically excludes failure as the result of a third party act or omission, misuse, unanticipated uses, fatigue failure or similar phenomena resulting from induced vibration, harmonic oscillation or resonance associated with movement of air currents around the product.

Specifications and dimensions subject to change without notice. Consult your Eaton's Cooper Lighting business representative or visit www.cooperlighting.com for available options, accessories and ordering information.

AMENDMENT TO CONDITIONAL USE PERMIT RESTRICTION AGREEMENT			
	<table border="1"><tr><td>Name & Return Address Town of Westport Thomas G. Wilson Town Attorney/Administrator 5387 Mary Lake Road Waunakee, WI 53597</td></tr><tr><td>Parcel Nos. 0809-123-8460-6</td></tr></table>	Name & Return Address Town of Westport Thomas G. Wilson Town Attorney/Administrator 5387 Mary Lake Road Waunakee, WI 53597	Parcel Nos. 0809-123-8460-6
Name & Return Address Town of Westport Thomas G. Wilson Town Attorney/Administrator 5387 Mary Lake Road Waunakee, WI 53597			
Parcel Nos. 0809-123-8460-6			

WHEREAS, that certain Conditional Use Permit Restriction Agreement was recorded on September 14, 2015 as Document Number 5183715 with the Dane County, Wisconsin Register of Deeds (the "CUP") providing for the terms and conditions by which a Winery may be operated upon the Premises legally described on Exhibit A attached hereto (the "Premises");

WHEREAS, the Town of Westport (the "Town") is satisfied that the Winery and related improvements petitioned for therein has been built in accordance with the approved plans and specifications therefore and has been operated in accordance with the terms and conditions of the CUP;

WHEREAS, the Town is satisfied that the Winery is otherwise in compliance with the CUP and finds the Winery to be a good steward and valuable addition to the economy of the Town;

WHEREAS, the owner of the Premises and the operator of the Winery hereby petitions the Town to allow for a change in the hours of operation of the Winery to comply with the State of Wisconsin's restrictions on hours of operation for wineries and certain other modifications to better serve its patrons;

NOW, THEREFORE, in consideration of the Recitals and for good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, this Amendment to Conditional Use Permit Restriction Agreement (this "Amendment") is entered into among the Town, the owner of the Premises and the operator of the Winery for purposes of modifying certain terms of the CUP, as follows:

1. The Town has inspected the Winery and agrees that construction of the Winery is in compliance with the terms of this CUP and its building permit.
2. Item 6 of the CUP is hereby modified by deleting the restriction on pole lighting. The operator of the Winery shall not add any permanent lighting in excess of that approved by the Town pursuant to the lighting plan without the consent of the Town. No such lighting shall be in use from one hour after closing time until 5:00 am. ; provided however, the operator of the Winery is expressly permitted to add Any low voltage landscape lighting, string lighting (including commercial grade) or other ambiance type lighting shall be subject to administrative approval by the Town. ~~without the consent of the Town.~~
3. Item 14 of the CUP is hereby deleted in its entirety and modified to read as follows:

The Tasting Rooms may be operated at all times permitted for wineries by Wisconsin Statutes. This CUP does not restrict the Winery's hours of operation.
4. Item 23 of the CUP is hereby removed.
5. Item 24 of the CUP is hereby amended to cause occupancy of the building to be restricted to the number of occupants permitted under the applicable fire code and parking requirements. All other items in the CUP referencing a specific number of occupants are also modified to be consistent with the modification of Item 24.
6. Item 25 of the CUP is hereby amended to allow outdoor music until close of business at the Winery, subject to the Village noise level restrictions.
7. Item 26 of the CUP is hereby deleted in its entirety and modified to read as follows:

Tents for events shall not be allowed without administrative approval ~~the express consent~~ of the Town, and no tents shall be allowed in the front yard.
8. The CUP is hereby amended to add a new Item ~~30~~²⁹ which shall read as follows:
Normal and customary marketing activities shall be permitted at the Winery as is similar with other wineries such as yoga classes, painting or craft classes, entertainment and other such activities.

Except as modified by this Amendment, all other terms of the CUP remain in full force and effect.

[Execution Pages Follow]

IN WITNESS WHEREOF, the parties below have hereunder set their hands and seals this _____ day of _____, 2019⁸.

OWNER:

DRUMLIN RIDGE FARM VINEYARDS LLC

By: _____
David J. Korb, Member

By: _____
Brenda J. Korb, Member

STATE OF WISCONSIN

COUNTY OF DANE

Personally came before me this _____ day of _____, 2019⁸, David J. Korb and Brenda J. Korb, to me known to be the persons who executed the foregoing instrument as members of and on behalf of Drumlin Ridge Farm Vineyards LLC and acknowledged that they executed the same with proper authority.

NOTARY PUBLIC, STATE OF WISCONSIN

Print Name: _____
My Commission Expires: _____

OPERATOR:

DRUMLIN RIDGE WINERY LLC

By: _____
David J. Korb, Member

By: _____
Brenda J. Korb, Member

STATE OF WISCONSIN

COUNTY OF DANE

| Personally came before me this ____ day of _____, 20198, David J. Korb and Brenda J. Korb, to me known to be the persons who executed the foregoing instrument as members of and on behalf of Drumlin Ridge Winery LLC and acknowledged that they executed the same with proper authority.

NOTARY PUBLIC, STATE OF WISCONSIN

Print Name: _____
My Commission Expires: _____

TOWN OF WESTPORT:

By: _____
Dean A. Grosskopf, Town Chair

Attest: _____
Thomas G. Wilson
Attorney/Administrator/Clerk-Treasurer

STATE OF WISCONSIN

COUNTY OF DANE

| Personally came before me this ____ day of _____, 2019⁸, Dean A. Grosskopf, Town Chair and Thomas G. Wilson, Attorney/Administrator/Clerk-Treasurer, to me known to be the persons who executed the foregoing instrument on behalf of the Town of Westport and acknowledged that they executed the same with proper authority.

NOTARY PUBLIC, STATE OF WISCONSIN

Print Name: _____
My Commission Expires: _____

EXHIBIT A

Lot Three (3) of Certified Survey Map No. 454 recorded in the Dane County Register of Deeds office in Volume 2 of Certified Survey Maps, page 248, as Document No. 1279320, in the Town of Westport, Dane County, Wisconsin. Subject to River Road.

This instrument drafted by:
Melanie S. Lee
Reinhart Boerner Van Deuren s.c.
22 East Mifflin Street, Suite 600
Madison, WI 53703



TOTAL PARKING STALLS = 66

DRUMLIN RIDGE FARM VINEYARDS – WINERY BLDG – TOWN OF WESTPORT

PARKING LAYOUT PLAN

PAGE: 1 OF 1

DATED: DECEMBER 20, 2018



4604 Siggelkow Road, Suite A – McFarland, Wisconsin 53558
Phone (608) 838–7750; Fax (608) 838–7752



WAUNAKEE/WESTPORT JOINT PLAN COMMISSION SUMMARY SHEET

MEETING DATE: 02/12/2019

ITEM: NEW BUSINESS No. 2

TITLE: Public Hearing and Discussion/Action on a Request by Wisconsin Scaryland to Extend the Original Conditional Use Permit for County Highway M for an Additional Two Years

ISSUE SUMMARY: The CUP previously approved in 2014 and is about to expire. The applicant is requesting a two year extension. This item was noticed as a public hearing.

STAFF RECOMMENDATION: Staff recommends approval.

RECOMMENDED MOTION: Approve the CUP extension for Wisconsin Scaryland for an additional two years.

ATTACHMENT(S): WI Scaryland Letter requesting extension.



WISCONSIN SCARYLAND
5803 County Hwy K



DISCLAIMER: The Village of Waunakee does not guarantee the accuracy of the material contained here in and is not responsible for any misuse or misrepresentation of this information or its derivatives. SCALE: 1" = 244'

VILLAGE OF WAUNAKEE
500 W. Main St
Waunakee, WI 53597
(608) 850-8500

Print Date: 2/5/2019

11/29/16

Dear Joint Planning Commission,

We would like to extend our use of 5305 Cty Hwy M in Westport for another two years per the verbiage in our agreement with you from three years ago.

We have successfully operated our Haunted House without any police calls, injuries or incidents. Zero. In three years, that is pretty amazing.

We attribute our success to our multi-faceted approach to safety and security. Our JBM patrol, parking enforcement, Security enforcement, security cameras, 2-way radio communications, professional training, and continued maintenance and upkeep of the buildings and grounds throughout the year.

When we were granted approval for the first 3 seasons, the Board and the Committee both felt that the limited 3-year approval was a good idea to see if our use of the property would pose a problem to the Town and residents, and that it was a "trial period" to see if our business was appropriate to the zoning and area.

We have continued to improve the property, clean up trash and debris, repair exterior issues, do snow removal, lawn care and tree trimming, and keep the corner from looking abandoned.

We have provided paid part-time employment to over 150 people, and paid full time employment to 4 people. We have supported Waunakee School clubs with fundraisers and the Fire Department with free tickets.

Additionally, the current owners of the property do not have any plans for redevelopment or sale within the next 3 years.

In December, we will begin working on next year's design, so we would like to get approval quickly to keep on schedule.

Thank you,

Guy Kitchell
Wisconsin Scaryland
guykitchell@gmail.com
608-287-6965



WAUNAKEE/WESTPORT JOINT PLAN COMMISSION SUMMARY SHEET

MEETING DATE: 02/12/2019

ITEM: New Business

TITLE: Westport Commercial Development Design Guidelines Discussion/Recommendation

ISSUE SUMMARY: The Town Design Guidelines Development Committee has viewed comments on the draft Guidelines document received as noted in their review plan, and incorporated comments as received and deemed necessary to make the document better. The Committee recommended approval at its last meeting, and is asking the Town Plan Commission to recommend approval to the Town Board. The JPC will should review the document and provide any comments they may have. Below is a link to the document.

https://waunakeepd-my.sharepoint.com/:b:/g/personal/tmeinholz_waunakee_com/EXHalQYCyqBMk2_rzgTJj64BK7WzuRPMT5zbKojzSmeRcQ?e=tKrAvF

STAFF RECOMMENDATION: Recommend to the Town Board for use subject to minor revisions as may be necessary to correct errors.

RECOMMENDED MOTION: Motion to recommend approval subject to minor revisions as may be necessary to correct errors.

ATTACHMENT(S): Final Draft Westport Design Guidelines.

TOWN OF WESTPORT

Commercial Property Design Guidelines



ACKNOWLEDGMENTS

TOWN BOARD

Dean Grosskopf, Chair
Terry Enge
Ken Sipsma
Mark Trotter
John Cuccia

DESIGN GUIDELINES DEVELOPMENT COMMITTEE

John Cuccia, Chair
Mark Trotter
Joe Pichette
Tom Wilson

TOWN PLAN COMMISSION

Dean Grosskopf, Chair
Cynthia Kennedy
Eileen Bruskewitz
Mary Manering
Chris Ohm
Joe Pichette
John Cuccia

TOWN STAFF

Thomas Wilson,
Attorney/Administrator/Clerk-
Treasurer
Jessica Frey, Executive Assistant

JOINT PLANNING COMMISSION

Ken Sipsma (Westport), Chair
Eileen Bruskewitz (Westport)
Dean Grosskopf (Westport)
Brian Malich (Waunakee)
Tim Black (Waunakee)
Brad Zeman (Waunakee)
Chris Thomas (Waunakee), Alternate
Tom Wilson (Westport Staff)
Kevin Even (Waunakee Staff)
Tracy Meinholz (Wauankee Staff)

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Introduction & Overview

The Town of Westport's (the "Town") Design Guidelines (the "Guidelines") have been developed to guide the appearance, form, and function of new development and redevelopment in nonresidential zoning districts within the Town.

In utilizing these guidelines, the applicant, the staff, or any other users should consider the characteristics of the site and its immediate context, applicable plans for an area, the nature of the use, and the intent of the guidelines and performance standards as they apply to the specific design district.

In cases in which special conditions exist that are not specifically addressed by the Guidelines, the design district intent statement should serve as the basis for determining the appropriateness of the proposed design.

Why do Guidelines?

Established in 1849 the Town is bordered by Lake Mendota to its South, the Yahara River to the East, the Village of Waunakee to the North, and the City of Middleton to its West. Much of the South border of Westport is Lake Mendota. The Yahara River, Six Mile Creek and Dorn Creek flow through Westport before entering Lake Mendota. The Town is home to three marinas and several boat sales and repair operations. Several boat and small craft landings are found in Westport. Westport is part of the greater Madison area and only minutes from the Dane County Regional Airport and State Capitol (by car or boat).

Westport has a rich history as a pre-settlement home to many Native Americans and is well known as having one of the largest concentrations of effigy mound in the United States. One of the earliest settlement roads was a converted Native American trail known as Military Road. This road connected two early military forts (Crawford and Winnebago).

The fertile land was ideal for early settlers, so the community has deep roots in farming and agriculture. The Town was home to the first trading post in the area as well.

How are the Guidelines organized?

The Guidelines are presented in six chapters:

- I: Design Districts**
- II: Site Planning**
- III: Architecture**
- IV: Landscape**
- V: Lighting**
- VI: Signage**

Each chapter starts with a set of goals that envision what the Town hopes to accomplish by adhering to the Guidelines. Individual chapters are divided into sections that deal with specific issues. For each issue the Guidelines provide planning objectives and specific design guidelines.

Photographs are used throughout the Guidelines to illustrate what may be considered acceptable in the Town. The photographs are representative samples to make the Guidelines more reader-friendly. The Guidelines are not meant to stifle creativity; in all situations there may be many ways to achieve the Town's goals.

It is in the public interest of the Town to attempt to preserve as much of this early history as possible.

Now, the Town is a mix of rural and urban uses, but with significant local parks and trails, and other preservation areas. These areas are linked or planned for linkage, which will enhance connectivity for bikes and pedestrians. The Town contains significant lands protected by Dane County and the State of Wisconsin, including Governor Nelson State Park. The Town has worked diligently to conserve open and agricultural lands, including community separation space, and has been very successful in doing so.

The intent of the Town's design guidelines is to respect and maintain the Town's "rural character" utilizing history to plan for the future. Every hometown needs a special place to gather and socialize with friends and neighbors. Westport is surrounded by Waunakee, Middleton, DeForest and Madison, yet its residents wish to maintain their own separate sense of place. These guidelines aid in maintaining that feel and enhance the rural hometown special place that is Westport.

What are the objectives of the Guidelines?

The application of design standards and guidelines to non-residentially zoned properties will assist the Town staff and plan commissions to implement the Comprehensive Plan and specific plans for the Town's business and industrial areas defined herein as the following Districts: Westport Town Center, Westport Commons and the High-Tech Business Park, as well as other neighborhood business districts in a more effective and cohesive manner.

The main objectives of the Guidelines, which will be used in guiding changes and development, and in reviewing plans for development and improvements, are as follows:

1. Assist the planning and design of quality development projects to provide for increased opportunities and land use efficiency for redevelopment and new development, and promote design continuity and cohesiveness at the main portal corridors entering the Town

of Westport.

2. Avoid piecemeal and fragmented development that detracts from the establishment of a viable business environment and strong neighborhood appearance and character that reflect plans or studies for the subject area.
3. Encourage a viable and compatible mix of commercial, business, office, and residential uses in the Town.
4. Encourage the integration of mixed uses in designated areas through the development of consistent building forms placed parallel to the street, with minimum setbacks from the right-of-way, and an attractive and viable pedestrian-friendly environment to the greatest extent possible.
5. Encourage creative planning and design in the arrangement and siting of buildings, parking areas, circulation and access, shared parking facilities and ingress/egress arrangements, and limit multiple curb cuts.
6. Make the policy of efficient traffic flow compatible with the goal of promoting an attractive and viable, pedestrian-friendly environment through the establishment of a safe, convenient, and attractive pedestrian and bicycle network of paths and public places.
7. Protect property and private investment.
8. Promote public health, safety and welfare.

The Guidelines are not intended to inhibit or restrict innovative and creative design solutions. Rather, the intent is to achieve a cohesive vision for development that appropriately applies techniques compatible with lasting styles and ideals that define the Town's rural character, historic charm, community values, and future growth. The Guidelines provide this essential regulatory tool for planning and design of developmental proposals by encouraging a sustainable and prosperous economic environment. These Guidelines complement and support policies already in place within the Town. The Guidelines share similar goals and objectives with various regulatory and planning documents, but focus on the building and spaces between them to create unique, compatible, comfortable, and safe environments. The Guidelines also assist the Plan Commission with the

implementation and enforcement of its primary function and responsibility - to protect the public's health, safety and welfare. The core objective of the Town's Comprehensive Plan is to preserve the Town's rural character. The Comprehensive Plan shall be used in conjunction with the Guidelines and will help ensure that long range land use and economic development objectives are achieved.

Note that the Comprehensive Plan may be revised from time to time. If the Comprehensive Plan is amended, then this document should be read to be consistent with any such change. If that is not feasible, then the Comprehensive Plan is the higher authority and controls over this document.

With this in mind, the following objectives shall be a priority for all development:

- Ensure prosperous economic development within a vibrant, traditional, and rural Wisconsin setting;
- Design to the human scale;
- Create architectural styles that are compatible with each other and harmonious with the surrounding architecture, rural character of the Town, and adjacent neighborhood fabric;
- Create a unique sense of place that promotes pride and social interaction among residents and visitors; and
- Ensure that the community is open and accessible to all Town residents and promotes a healthy lifestyle.

Past approved designs may provide for some guidance, if somewhat recent and not based on past approvals. For instance, design concepts for Yahara Crossing, The Athens Grill, and related convenience store predated recent approvals and would not pass muster under this document or current standards. The same is true for several Skipper Bud's metal storage and marine structures. The Boat House, Kraemer Printing and the former Gary Stone's Paddle Board Place are in similar stead. The Summit Credit Union, Pine View Veterinary Clinic and Pet Retreat, and Inspire Day Care structures, as well as those approved in the JPA along Century Avenue for the Kilkenny Farms development, are more indicative of these Guidelines.

Are the Guidelines mandatory?

Throughout the document the word "should" is used to denote that these are recommended guidelines and not mandatory standards. The Guidelines in this manual are intended to accompany adopted Town plans and guide the Town staff and commissions during the review process of applications from business or property owners for improvements, and modifications to existing buildings and for parking and circulation improvements in the commercial districts. In many cases the Town plan commissions encourage compliance in order to facilitate the review of development projects.

Where do the Guidelines apply?

The provisions of the Guidelines apply to all nonresidential zoned areas in the Town. They apply to new construction as well as expansions or redevelopment of existing buildings and sites. They will apply to isolated commercial developments located outside of Design Districts referenced below, and the provisions below will be applied to those developments at the approving review body.

How will the Guidelines be used?

There are two main functions of the Guidelines. First, they will provide guidance to landowners and developers in the early stages of planning and design, to address this question: "What is the Town looking for?"

Secondly, they will be used as a benchmark by the staff, consultants, commissions, committees, boards, and peer reviewers to evaluate development proposals as part of the review processes to address the questions of "Does it meet the Town's criteria?" and "What will it look like and how will it function?"

Implementation of the Guidelines may rely on the services of architects, civil engineers, and landscape architects working as consultants or developers. The Guidelines will be administered by staff through the review process. The Guidelines will be applied to development that requires site plan approval from the Town plan commissions.

What Zoning Authorities are in play and how is that applied?

The Town has several authoritative documents dealing with design, and is the town included in three different zoning districts with differing code sections, but similar concepts. The High-Tech Business Park and Westport Commons Districts are all included in Waunakee's zoning jurisdiction where the Waunakee Zoning Code applies (as does the Waunakee Land Division Code as the law provides). The Westport Town Center District is partially within Waunakee's zoning jurisdiction (the area north and west of the Yahara River), but also a small area is under the Town Zoning Code (south and east of the Yahara River). The Waunakee Land Division Code applies as provided by law in this District. The Waunakee-Westport Joint Comprehensive Plan, including the adopted CORP, essentially is in place across the entire Town, having been approved for such use by Middleton, Waunakee, Westport and Dane County. The Town Land Division Code does apply in all Districts and all across the Town.

Attached at Appendix A is a bibliography of these zoning and planning ordinances, regulations, jurisdictional maps, and links.

Are there specific architectural themes to utilize?

Based on the Town's history, uses, current geography, and Comprehensive Plan, three types of themes will be considered for structures in the Town: Architectural theme, Prairie Architecture (Usonian or New Urbanism for example), or an Architectural/Farm related architectural theme. The areas along the Yahara River and Lake Mendota in the Town Center should follow the Nautical type theme. In the other Districts, developers should propose one or more of the themes and explain how they will best fit the area and neighboring uses. Example pictures that follow in the design specifics include renderings or photos of these types of architectural themes.

What will the ultimate outcome be for the Town?

The Guidelines are not designed to produce immediate results. Like the Comprehensive Plan and other Town plans, they provide a framework for the future. The process is intended to ensure that site plans are reviewed efficiently by plan commissions and staff resulting in high quality development that improves the Town's overall aesthetic cohesiveness and immediate environment of the subject site.

I. Design Districts

Introduction

Three separate “Design Districts” have been developed for non-residential commercial, business and industrial areas within the Town. Each of the Design Districts have their own particular design characteristics and architectural vocabulary as defined later in this document. All three of the Design Districts border gateway corridors to the Town in some fashion.

Districts Established

The more urbanized portion of the Town has been developed along a T-shaped corridor following the STH 113 and CTH M intersection westerly to CTH K. This area has been described in the Comprehensive Plan as the “Westport Town Center.” Map 1 attached shows the Town Center Commercial areas from the Comprehensive Plan. There are three distinct commercial areas each which has unique locational and geographical features lending toward necessary design and site plan distinctions. Those areas are shown at Map 2. Those will be identified as the High-Tech Business Park District, the Westport Town Center District, and the Westport Town Commons District.

High-Tech Business Park District

This District is essentially the Business Park area which will remain in the Town when developed located North of CTH M and West of STH 113 (Kennedy Drive to W. River Road). This area is a flat and open area, currently farmed, and mainly owned by one company looking for high energy consumers. Its location along STH 113 close to the North entrance of Madison, and a short distance from both the airport and the Interstate system, make it very attractive for technology based businesses and manufacturers. This District is a unique asset in the Town. A substantial portion of the District is currently a Certified Site through the Wisconsin Economic Development Corporation, and is one of only twenty sites in the State designated as a shovel-ready development intended to increase employment and tax base. The Comprehensive Plan promotes this District as industrial and commercial in nature. As such, this District is likely focused on a small number of larger users.

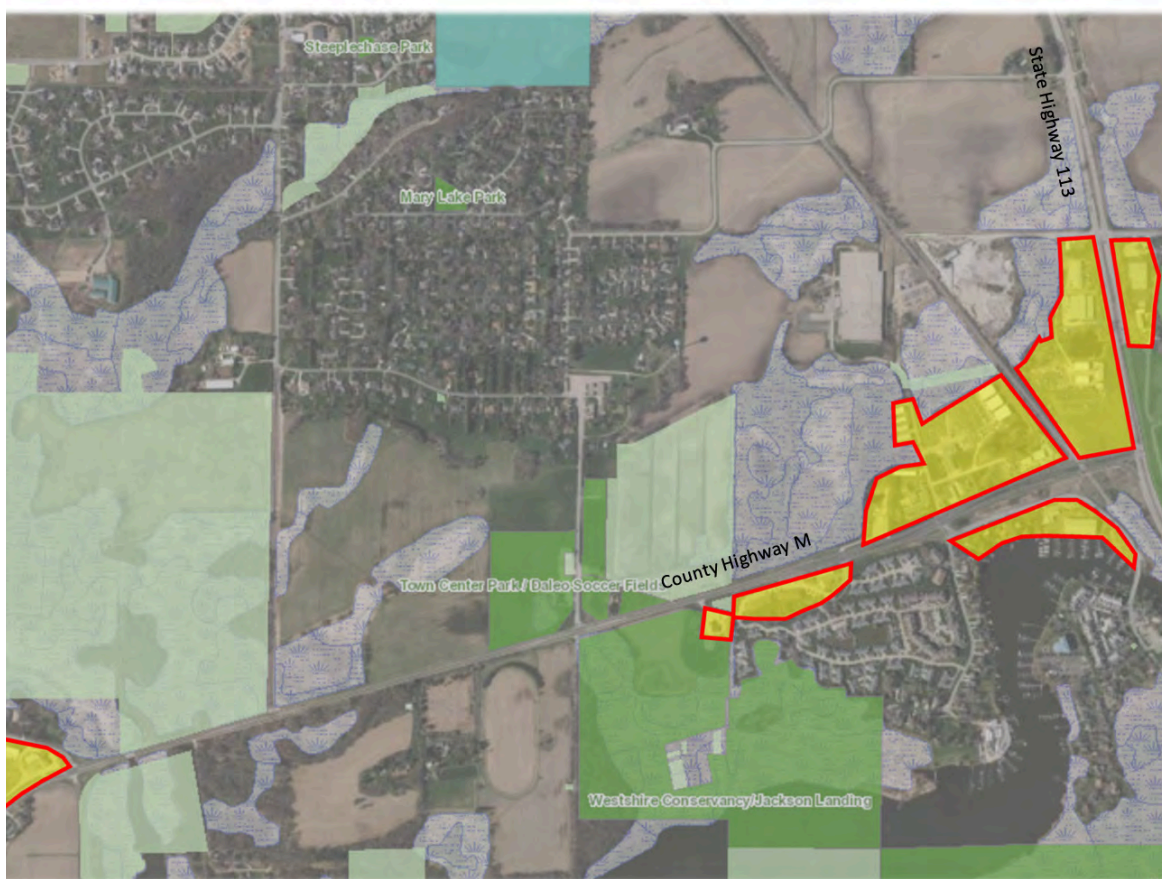
Additionally, acknowledging the unique nature for potentially larger manufacturing or industrial users and structures in this District, it is understood that some of the limits and design elements expressed in this document may need to be adjusted in the opinion of the Town Plan Commission and/or Town Board depending on these proposed uses and the number and size of structures included in this District. For instance, if only one or two users utilize the property in this District with larger structures anticipated, the Town Plan Commission and/or Town Board may adjust more of the recommendations included here as opposed to if there are several users and structures anticipated (allow more guideline flexibility for larger projects specifically in this District).



High-Tech Business Park Boundary Map

Westport Town Center District

This District is essentially the Business Park area which will remain in the Town when developed located North of CTH M and West of STH 113 (Kennedy Drive to W. River Road). This area is a flat and open area, currently farmed, and mainly owned by a utility holding company looking for high energy consumers. Its location along STH 113 close to the North entrance of Madison, and a short distance from both the airport and the Interstate system, make it very attractive for technology based businesses and manufacturers.



Westport Town Center District Boundary Map

Westport Commons District

Westport Commons District. This District is established covering the area along CTH M from Mary Lake Road to Woodland Drive, including the Town Center Park and facilities. This area is unique in its link to several Westport and Waunakee residential neighborhoods north of the area to the businesses and recreational opportunities in the Town Center District. It is sandwiched between Six Mile Creek and its protective parklands and trails, and the Town local government center and active park. Other than the Town government area, the area is owned by one party, and contains environmental corridors and unique connectivity options. This area is unique as it has significant potential for a special gathering place in the Town. Should this area be developed, it is the desire of the Town for this area to maintain a rural character. This area is part of an interconnected trail system and part of the regional biking and hiking trail system. This area contains significant wetlands which are connected with more wetlands located to the west and south. The northern portion of this area is bordered with single family homes, and to the east are the Town Center government structure and Daleo Soccer Fields. This area would be ideal for a mixed use gathering place with local shops, restaurants, civic uses, and entertainment, while combining the need for a gathering place, with sitting areas, small parks, trails for biking and walking, allowing nearby neighborhoods pedestrian access.



Westport Commons District Boundary Map

Note that the Town believes that the Westport Commons District will become the hub for the Town commercial areas and residential neighborhoods. It will be a new and major connection point for commerce, traffic, multi-model travel, and recreational activity on the north side of Lake Mendota. It is vital that the Town be patient and plan this area as a whole, integrating it to Lake Mendota, Six Mile Creek, the Yahara River, highways, parks, trails, government, recreational facilities and pedestrian/bicycle trails that all meet at that point. There is no room for error or mistake because to do so could last life times. The High-Tech Business Park similarly should be looked at as a whole and will transition from STH 113 to the Mary Lake neighborhood. Town Center District is similarly situated, but not a blank slate, so care must be taken to not continue older bad habits, and integrate new buildings and designs as they are proposed.

These three areas will have many design guidelines in common, but also will require unique policies based on the differing expectations of ultimate development. As noted previously, some isolated commercial developments outside of these Districts occur in the Town from time to time. The appropriate reviewing body will apply the provisions below which are deemed appropriate for the location and type of development in the discretion of the reviewing body. The following sections will further detail these common and unique guidelines.

General Design Standards from Current Town Regulations:

i. Common Guidelines. The following policies generally guide design and property development in all of the Districts.

A. Any new use should not threaten the natural environment in any way, including an increased risk of contamination to the air, soil, surface waters, or ground water; increased sound levels; or increased nighttime light levels.

B. Any new use should be compatible with existing adjacent uses, especially any residential uses. Buildings and sites should be designed to

prevent noise, odors exterior lighting, or traffic patterns that would be objectionable to existing adjacent uses.

C. Any use that generates excessive traffic, does not create good paying jobs for local residents, uses dangerous or potentially hazardous types of processes, requires large amounts of raw material, generates significant amounts of waste products, utilizes or requires hazardous materials, generates excessive noise, or has the potential to negatively impact the natural and agricultural resources of the Town should be prohibited.

D. Any lighting on commercial structures should be recessed and/or screened so that there is no spill-over onto adjoining properties. Lighting should be permitted only as allowed under the Town's Dark Skies Code.

E. Commercial developments should meet all signage and parking requirements of the applicable codes, unless granted a waiver or variance as afforded by those codes.

F. Any commercial use that would be incompatible with neighboring uses, especially rural residences and farming operations, should be prohibited. Incompatible uses may exhibit one or more of the following characteristics:

- Excessive noise, light, traffic, or odor.
- Dissimilar architecture, signage needs or other appearances.
- Requires excessive employees or generates significant customers from outside the Town of Westport or immediate area.
- Requires exceptional levels of public improvements or services.
- Lack of appropriate area for setbacks or screening between neighboring buildings, parcels and other land uses.

G. Multi-unit residential uses may be considered on a case-by-case basis, either as a stand alone use or mixed with a commercial use. The suitability of such use will be determined by site characteristics and the quality of the design. The applicant needs to demonstrate the safe provision of access, parking, and pedestrian routes and show how the residential use relates to other uses on that site or adjacent sites.

H. Multi-family housing, if allowed, should be well designed and arranged within the site integrated amongst surrounding lower-density housing units and sites, and provided with the proper level of utilities and community services.

I. Encourage commercial and residential uses with shared walls be constructed with adequate sound proofing and durable materials to reduce conflicts and operations and maintenance costs.

J. Development should connect to or establish a connected street, sidewalk, recreational path and trail network that promotes walking, biking, and transit in addition to motor vehicles, and that connects to regional trails, recreational paths and roadways in surrounding areas, creating a highly connected travel network for pedestrians, bikers, transit, vehicle, and other travelers.

K. Each District should contain public access to outdoor recreational activities.

L. Design street and trail networks to provide convenient walking and biking access for residents to civic, business, and recreational uses.

M. Leverage recent investments in trails and proposed park investments to build community image and enhance development potential and quality in the District.

N. Appealing opportunities should exist for youth to be productive and engaged community members.

O. When new proposals are considered, evaluate existing development to determine where connectivity can be improved.

P. Work with Dane County, State, and Federal governments to link recreational trails, and identify gaps in trails and bikeways.

Q. Accommodate transit (traditional and para-transit), ride-share, walking, biking, and other non-vehicular forms of travel to accommodate persons of different ages and abilities.

R. Establish interconnected transit, bike, and pedestrian networks between the FUDA North Mendota Communities and neighboring communities.

ii. Unique District Guidelines. The following policies generally guide design and property development in the Districts established here.

A. High-Tech Business Park District

This District is near the primary entrance corridors to the Town Center and should be attractive and inviting along STH 113.

1. Buildings and landscaping along the main corridors should be aesthetically pleasing.
2. Corridors should be improved by public improvements such as lighting, signs and landscaping in the public right-of-way.
3. Development along these primary entrance corridors requires special attention and design effort in the development process to ensure strong and attractive entrances. This should include the selection of high quality materials and design for buildings, exceptional landscaping and buffering techniques to obscure the view of features not intended for view from a highway, such as residential rear yards, garage doors and loading docks.
4. Implement streetscaping and community design/character guidelines. Attached at Appendix B are examples of general architecture and site appearances to be considered in this District.
5. Given that this District is targeted towards industrial and commercial development and has a focus on employment, the following may be allowed in this District:
 - Building features different from the Westport Town Center District and Westport Commons District;
 - Buildings that may be taller than in other districts and have an architectural style befitting an industrial/commercial

development which may differ from Prairie Architecture, Agricultural/Farm or related themes;

- Designs which accommodate truck and train access and movement;
- Businesses and buildings with noise levels different from the lower levels expected in the Westport Town Center District and Westport Commons District;
- Businesses and buildings that generate traffic and may use a variety of manufacturing processes which require or process raw material;
- Parking lot siting and design that differs in size and location from other Districts to accommodate semi-truck or rail spur access and loading;
- Utilities and mechanical equipment to be placed at grade as long as they are placed in safe manner, location and with reasonable screening;
- Sidewalks and pedestrian spaces tailored to the employees of the businesses in the District, and primarily used as internal sidewalks for safe passage to and from the parking lots;
- Storage, loading/unloading, loading docks, dumpsters, recycling, mechanical equipment, fuel areas, and service areas to be placed to cohesively fit into the site while providing for functionality of building design, orientation, and positioning;
- Site plans orientated in such a way to best suit development and business operations with screening as appropriate;
- Buildings located on the corner of two public streets not close to the corner as site plans may vary to best complement efficiency and productivity;
- Company corporate colors and design standards;
- Parking lot landscaping in line with landscaping typically found in industrial settings with planting strips and shrubs/ ornamental plantings as standard for the type of business; and,
- Some incompatibility with the three multi-family residential buildings in an A-1 zoned area north of the District property at the corner of STH 113 and Kennedy Drive.

B. Town Center District

1. This District contains the primary entrance corridors to the Town Center and should be attractive and inviting along STH 113 and CTH M.
2. Buildings and landscaping along the main corridors should be aesthetically pleasing.
3. Corridors should be improved by public improvements such as lighting, signs and landscaping in the public right-of-way.
4. Development along these primary entrance corridors requires special attention and design effort in the development process to ensure strong and attractive entrances. This should include the selection of high quality materials and design for buildings, exceptional landscaping and buffering techniques to obscure the view of features not intended for view from a highway, such as residential rear yards, garage doors and loading docks.
5. The Town should consider similar specific designs for development along the entrance corridor highways as in Waunakee entrances.
6. Prohibit commercial driveway access directly onto CTH M and STH 113, and where feasible, use shared driveways, frontage drives, and access from side streets.
7. Consider higher residential and commercial densities and mixed-use development along proposed transit corridors, and in redevelopment infill projects, such as along the Yahara River, including potentially as proposed in the Yahara River Redevelopment project (created as a capstone project by a UW landscape architecture student).
8. Consider higher density residential development in and around commercial centers to build community and strengthen the customer base for local businesses.

9. In general, allow for greater commercial and residential density and mixing these uses near existing and future destinations and transit corridors (such civic uses, entertainment venues, retail) to encourage walking, biking, and when feasible, transit.
 10. Permit senior residences (life-cycle housing), multi-family housing, and homes with a broad mix of sizes and price-points, including homes accessible to people with disabilities.
 11. Mix housing, civic, open/public spaces, retail and office uses, horizontally and vertically.
 12. Implement streetscaping and community design/character guidelines.
 13. Take advantage of significant potential for outdoor recreation in this District with the Yahara River, Six Mile Creek, Lake Mendota, and Daleo Soccer Fields/Westport Town Center Park and connect them to all businesses and residents in the District, limited by the fact though that natural resources are finite, and so sustainable use of resources is necessary to ensure current and future use.
 14. Attached at Appendix C are general examples of architecture and site appearances to be considered in this District.
- entertainment venues, retail) to encourage walking, biking, and when feasible, transit.
 4. Permit senior residences (life-cycle housing), single-family lots, multi-family housing, and homes with a broad mix of sizes and price-points, including homes accessible to people with disabilities.
 5. Mix housing, civic, open/public spaces, retail, and office uses, horizontally and vertically.
 6. Take advantage of significant potential for outdoor recreation in this District the Town Center area with proximity to the Yahara River, Six Mile Creek, Lake Mendota, and Daleo Soccer Fields/Westport Town Center Park, and connect them to all businesses and residents, limited by the fact that natural resources are finite and so sustainable use of resources is necessary to ensure current and future use.
- Attached at Appendix D are general examples of architecture and site appearances to be considered in this District.

C. Westport Commons District

1. Consider higher residential and commercial densities and mixed-use development along proposed transit corridors.
2. Consider higher density residential development in and around commercial areas in this district to build community and strengthen the customer base for local businesses.
3. In general, allow for greater commercial and residential density and mixing these uses near existing and future destinations and transit corridors (such civic uses,

18 I. Design Districts

II. Site Planning

Introduction

Each property is unique. Plans for development and redevelopment should be based upon a careful understanding of the site and its surroundings in order to meet the requirements of the ultimate user, while meeting the Town's goals for functionality, safety, and visual character, and ensure consistency with adopted plans.

These guidelines are intended to supplement, illustrate, and amplify various applicable sections of the Town code and comprehensive plan as noted.



Site Planning Goals:

- Distinctive, attractive properties that welcome people to the Town.
- Developments should be integrated into the commercial areas and the immediate context through street connections, sidewalks, connecting outdoor spaces, land use transition and compatibility, and building scale and character, which respects the uniqueness of each property and reinforces the Town's sense of place and character. Seek a balance between serving both automobile and pedestrian movement.
- Public open space throughout the Town to enhance its appearance and support pedestrian use.
- An attractive, functional, and safe environment that is conducive to commerce and other permitted activities.
- Protection for abutting residential properties through sensitive site planning, buffering, and architectural design.
- Upgrading visual character and sense of human scale in spaces through particular attention to architecture, site planning, signage, landscaping, and lighting.
- Encourage increased walking and bicycling by providing safe, attractive, interconnected facilities.
- Universal accessibility that meets the Americans with Disabilities Act (ADA).

General Site Planning Principals

Objectives

Good site planning should result in an attractive, safe, and economically viable relationship between buildings, parking, signage, lighting, landscaping, and the surrounding environment. Site plans should minimize the visual effects of parking and utilities, feature high-quality landscaping, accommodate pedestrian and bicycle movement, and encourage connections to nearby properties.

Design Guidelines

- **Site Analysis.** The site plan should be based upon a careful analysis of existing site conditions that considers topography, wetlands, soil conditions, existing vegetation, drainage, abutting land uses, and other factors that will influence the placement of buildings, roads, and parking areas. The plan commissions and boards may require a graphic presentation to demonstrate how knowledge of site conditions has influenced the site plan.
- **Preservation of Existing Features.** Site development should minimize disruption to natural and cultural features (e.g., mature trees, wetlands, drainage ways, stone walls) in a manner that would change their existing character.
- **Open Space.** Open Space areas should be preserved and integrated throughout the development. Where possible, open space should be continuous and used to preserve significant natural and cultural features. Open space should be coordinated with abutting properties to create continuous open space networks for wildlife corridors, riparian buffers, visual screening, etc.
- **Use of Open Space.** Open space should not contain any type of commercial activity, overflow parking, paved surfaces, constructed stormwater management facilities, or active recreation. Uses may include open vegetated areas, picnic areas, planting beds, bioretention areas, naturalistic water features, and similar features.
- **Parking Lots.** Parking should be located primarily at the side or rear of the building, with minimal parking in front. Parking lots should be screened to minimize their appearance in all districts, and need to maintain all stormwater runoff. Porous surfaces could be considered.
- **Relationships to Surrounding Properties.** Developments should be linked with the adjacent properties in the district and surrounding areas in order to provide direct, safe, and convenient pedestrian, automobile, and bicycle access. Where applicable, the expansion of the pedestrian network should be achieved through the: extension of public and/or private streets whenever possible, extension of sidewalks and/or paths in and through the development (such as those identified in the Dane County Park and Open Space Plan and Regional Trails Map), and extension of green space in and through the development.
- **Ancillary Uses & Utilities.** Service areas, outdoor storage and sales areas, HVAC equipment, trash containers, and other similar features should not abut residential neighborhoods and should be screened from adjacent properties and streets.
- **Buildings in Existing Parking Lots.** The development of buildings on out-parcels or additions extending towards the street are strongly encouraged to break up the scale of large parking areas.
- **Coordinated Future Development.** Where site plans are presented for a portion of a property, the applicant should show how the plan has been designed to accommodate future buildings, access roads, sidewalks, drainage, utilities, signage, and preserved open space in a coordinated fashion.

- **Orientation of Development.** While the majority of the existing buildings are free-standing and are not situated at close proximity and parallel to the roadway, the redevelopment of these properties should consider the building placement as it relates to roadways and the adjacent properties.
- **Entryways.** The design and character of the entryway to the development establishes its identity and its theme and sets forth its image and quality. Entryways should be designed in a manner unique to the character and theme of the development and should also be compatible with the existing general character of the adjoining areas in the district.

- **Corner Sites.** Site entryways, primary circulation patterns, and connections to adjacent uses should act as complete streets and serve pedestrians, bicycles, automobiles, and other modes of transportation without the necessity to use arterial roadways where possible. Street extensions, roadways, and vehicular drive connections to adjacent developments should include facilities to accommodate pedestrians and should incorporate streetscapes or landscaping. New access points and roadways used for site entry should align across primary roadways to the greatest extent possible forming controlled intersections.



Redevelopment of this site establishes several more pronounced entryways to the building and draw a stronger connection towards the adjacent roadway

- **Above Ground Utilities.** Utilities traditionally place above ground (electric, telephone, cable, etc.) should be examined for potential to: be placed underground, placed behind buildings with rear connections or consolidated on as few utility poles as possible. Substations, transformers and fuel tanks should be placed in screen enclosures away from primary pedestrian paths and vehicle entry areas.

Circulation

Objectives

Development activities should be characterized by safe, user-friendly, and efficient traffic flow. Access management principles should be followed to reduce the number of curb cuts, provide a safe vehicular and pedestrian environment, encourage intra-parcel travel, and minimize the number of trips on roadways

Design Guidelines

- **Curb Cuts.** Site plans involving curb cuts should comply with Town or other applicable requirements and plans should demonstrate an adherence to sound access management principles to promote efficient traffic flow and maintain a high level of safety for pedestrians, bicyclists and motorists. The number of curb cuts should be minimized to increase vehicular and pedestrian safety and the location and design of driveway entrances to the property should minimize conflict with off-site traffic, and provide for safe transition into the parking lot. Entrance drives should be located so that they reduce potential traffic problems, afford maximum sight distance, provide adequate queuing, and acceleration and deceleration lanes if necessary.
- **Shared Access.** Entrances to abutting commercial properties should be combined wherever feasible to minimize curb cuts and provide for more efficient traffic flow across developments.
- **Internal Traffic Flow.** To ensure the safety of motorists and pedestrians, the development plan should clearly delineate internal traffic patterns for both vehicles and pedestrians. Parking spaces, directional arrows, crosswalks, raised curb landscaped islands, and other markings on the ground should be delineated with pavement paint/material signage.
- **Connections with Adjacent Properties.** Pedestrian and vehicular connections between parking lots and driveways on adjacent parcels should be provided wherever feasible to minimize turning movements onto major roadways. Internal connections should provide safe, direct access while discouraging vehicular shortcuts. Cross easements should be provided as required to facilitate circulation and anticipate future connections.
- **Traffic Calming.** New developments or modifications to existing buildings should ensure their integration with the existing and future traffic patterns and traffic volume through the use of appropriate traffic management techniques. Traffic calming measures should be included where appropriate to discourage speeding within the site and between abutting properties. Measures may include speed tables, on-street parking, raised crosswalks, vertical curbing, curvilinear road alignment, roadside plantings, neck-downs, curbed islands, and signage.
- **Drive-Throughs.** Where such uses are allowable, access routes leading to or from takeout windows or other drive-throughs should minimize conflicts with pedestrian circulation routes. Motorists should be made aware of pedestrians through signage, lighting, raised crosswalks, changes in paving, or other devices.
- **Pedestrian and Bicycle Movement.** The development plan should provide for safe pedestrian and bicycle movement within the site and between abutting properties. Walkways should be designed to create a safe, efficient, and uninterrupted pedestrian way, and walkways should avoid frequent crossings by driveways or streets. They should be separated from streets and parking lots by curbs or other means to create physical separation. Specifically, onsite pedestrian connections should be provided to and between the following points:
 - The primary entrance or entrances to each building.

- Existing or planned transit stops, stations, and park-n-ride locations.
- Existing or future trail systems or amenities, where determined appropriate by the Planning Commissions, Town Board or Joint Planning Commission.
- **Maintenance.** All crosswalks and parking area lines should be repainted periodically to ensure their effectiveness.
- **Transit Connections.** Redevelopment and/or site improvements should plan for access and connectivity to existing and future transit facilities, including, but not limited to, park-and-ride locations, pullouts, stops, and shelters. Transit stops should be incorporated into the layout of the site, and should be protected from automobiles, where feasible, to become safe pedestrian spaces.



Elements such as stripping or pavement patterns can be used for aesthetics as well as traffic calming

Parking Area

Objective

Parking lots should be designed to complement adjacent buildings, the site, and the design district without becoming a dominant visual element. Every effort should be made to reduce the scale of parking lots by minimizing the total amount of paved surface visible from the road.

Providing more parking than the code requires adds substantial costs to development and redevelopment, and in some cases the added costs will prevent development altogether.

Parking lots should be designed as inviting, pedestrian-friendly places by careful attention to landscaping, lighting, and walkways. Parking areas should also comply with applicable code requirements, comply with the Town's stormwater standard, and consider using porous surfaces.

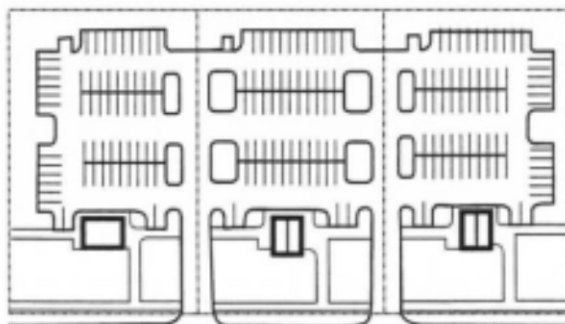
Design Guidelines

- **Orientation.** Parking lots should be designed as part of the overall plan for the site, and coordinated with the circulation plan, building entrances, lighting, landscaping, snow storage, and service areas. Parking lots should be located and designed so they do not detract from the character and scale of the surrounding area and the streetscape, and where feasible, the majority of the parking area should be located to the rear and sides of the building so that they can be screened by buildings.
- **Scale.** Parking areas should be broken up with trees, landscaped islands, grade changes, low walls, or other appropriate features. Large expanses of uninterrupted pavement should be avoided and new parking areas should incorporate green infrastructure facilities to accommodate runoff.
- **Shared Parking.** Shared parking use among different sites, where the peak parking demands occur at different times, is encouraged as allowed by code.

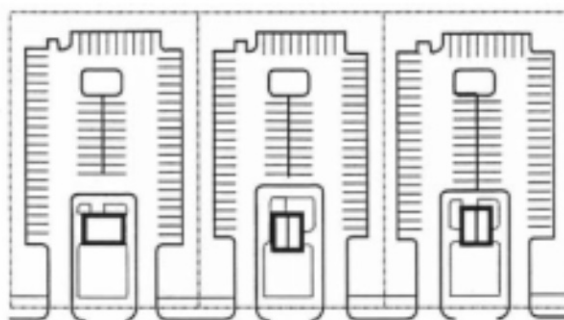
- **Parking Aisles.** Parking lots should be oriented to minimize the number of parking lanes crossed by pedestrians.
- **Sustainable Parking.** Parking areas constructed with sustainable materials in accordance with LEED or similar design specifications can be used for infrequent parking or overflow parking.
- **Parking Obstruction.** Pedestrian walkways through parking areas should allow for at least a four foot (4') wide clear zone from vehicles obstructing the walkway.

Shared Driveways

Do this



Not this



Pedestrian Spaces

Objectives

Commercial buildings should provide outdoor spaces for a variety of uses – seating/resting, dining, displays, and aesthetic enhancement – to create a pedestrian-friendly environment. Decisions with respect to the pedestrian movement system should be considered concurrently with the site planning process, and should not be added as an afterthought in the form of pavement strips, left over space, or orientation signs.

Design Guidelines

- **Outdoor Spaces.** Development plans should include outdoor use areas such as greens, plazas, and courtyards appropriate to the use of the property. Buildings should be oriented toward open spaces rather than roadways, and should have a major access on the space. Outdoor spaces should be coordinated with the pedestrian circulation plan to encourage pedestrian use, with provisions for seating and outdoor activities as appropriate. Outdoor spaces should be designed to separate pedestrian and vehicular traffic with landscaping, grade changes, and other site features.
- **Planning.** Where outdoor use areas are provided, they should be located in sunny, highly visible locations and sized to fit the anticipated uses.
- **Materials.** Outdoor use areas should be constructed of high quality, easily maintained materials. All elements within the space should be coordinated with the architecture and site elements to achieve a unified look. The use of decorative paving is encouraged for sitting areas, pedestrian plazas, building entrances, or other designed open spaces.



Public Sidewalks

Objectives

Public sidewalks and planted esplanades can be a highly desirable part of the streetscape, adding scale in a commercial landscape and creating a safe place for pedestrian movement.

There are many areas in and around the Town which are currently not pedestrian or bicycle friendly. The long-term objective is to provide an interconnected network of bike paths and sidewalks, consistent with applicable plans, that provide an alternative to the automobile and encourage exercise for the general population.

Design Guidelines

- **Coordination with Other Sites.** Facilities should be coordinated with abutting land uses to create interconnections throughout the commercial area and linkages to surrounding residential neighborhoods. Lighting and other amenities abutting walkways should be at human scale and provide line of sight to other pedestrians, motor vehicles, etc.
- **Coordination with Site Plan.** All new sidewalks should be coordinated with the Site Plan to avoid conflicts with landscaping, utilities, grading, drainage structures, signs, and other elements.
- **Material Selection.** Materials selected for curbing and sidewalks should be durable and long-lasting, and consistent with the character of the design district.
- **Crosswalks.** Where sidewalks intersect with commercial drives or roads, crosswalks should be installed to alert the motorist and improve visibility. Crosswalks should offer a noticeable change in texture and color. Raised crosswalks should be considered at key locations as a traffic calming device to make crosswalks more visible. Signs may be warranted in certain situations as determined by the appropriate codes.



Internal Walkways

Objectives

Site development should consider the needs of the pedestrian for safe, functional, attractive walkways throughout the property.

Design Guidelines

- **Location.** Internal walkways should be located where motorists can anticipate pedestrians and react accordingly. Walkways should be designed to give the pedestrian a full view of oncoming vehicles, with minimal interference from trees, shrubs, signs, light fixtures and parked cars. Walkways should avoid drive-through lanes, access and service drives, and other high-traffic routes.
- **Orientation.** Walkways in parking lots should be aligned with the main entry or a focal point on the building to assist in wayfinding.
- **Width.** Internal walkways should be a minimum of four feet wide. Additional width may be necessary in certain conditions, e.g., where shopping carts may be used, where heavy pedestrian traffic is anticipated, or where cars overhang the walkway.
- **Coordination with Landscaping.** Areas adjacent to walkways should be landscaped with trees, shrubs, flower beds, ground covers, or other such materials for year-round interest. Shrubs should be used with care to avoid blind spots. Special features, such as benches, flower beds, planters, and artwork can be used to enhance the walkway. Trees along all walkways should be trimmed to provide adequate sight distance and to remove potential obstacles.



A dedicated walkway that provides a safe, well-marked pathway to a main entrance while minimizing conflicts with vehicles

Multiple Building Developments

Objectives

Developments consisting of more than one structure should exhibit a high degree of coordination in site planning, architectural design, site design, and site detailing. All physical components should be designed to complement an overall plan.

Design Guidelines

- **Master Plan.** Where multiple buildings are proposed, a master plan should be prepared to show the general location of future buildings, parking lots, roads and driveways, lighting, signage, landscaping, walkways, utilities, service areas, stormwater management, and other components of site development. The master plan should also show how traffic, stormwater, and utilities will be coordinated with adjacent properties. The plan should consider significant natural or cultural features and integrate open space.
- **Phasing Plan.** As part of development plan applications, the applicant should provide a phasing plan that illustrates the sequence of development and what steps will be taken to ensure compatibility between current and future activities.
- **Building Orientation.** Multiple building developments should be designed to create usable, safe and attractive pedestrian spaces, at a “human scaled”, and preserve significant site features, and minimize the appearance of parking areas.
- **Focal Points.** A limited number of buildings or other elements should be designed as focal points. These structures should be visually more prominent, enhanced by height, massing, distinctive architectural treatment, lighting, landscaping, or other distinguishing features.
- **Circulation.** A unified site plan will be required that will identify ingress/egress, internal circulation, and shared driveways that should be installed, to the extent possible, with the first phase of development.



Outdoor Service & Storage Areas

Objectives

Outdoor service and storage areas should be integrated into the overall site plan. They should be designed to meet the functional needs of the facility while minimizing any traffic or visual conflicts, audible noise, or smells.

Design Guidelines

- **Locations.** All facilities for service, including waste collection and storage facilities/areas, off-street loading and unloading areas, loading docks, utility areas, mechanical equipment, dumpsters, fueling areas, and vehicle service and maintenance areas should be located at the side or rear of the principal building. Locations that face public roadways or abutting residential properties should be avoided.
- **Design.** Outdoor service and storage areas should be sized to fit the specific needs of the building and its intended operations.
- **Screening Design.** Service areas should be screened with architectural elements such as walls or fences. Screening may be further enhanced with evergreen trees, shrubs, and earth berms. Structural screens and fencing should complement the design of the main structure by repetition of materials, detailing, scale, and color. Where chain link fencing is required for safety, it should be landscaped and painted black or a similar dark color, or coated with dark vinyl. All screening shall be properly maintained.
- **Recycling Facilities.** The installation and use of recycling bins is encouraged. All recycling facilities should be screened in a manner similar to other service areas. Dumpsters and recycling areas should be consolidated where possible.



This service area is screened by a solid wall that repeats the design elements used elsewhere on the site



Trash enclosures should be sized to accommodate the dumpster for the facility

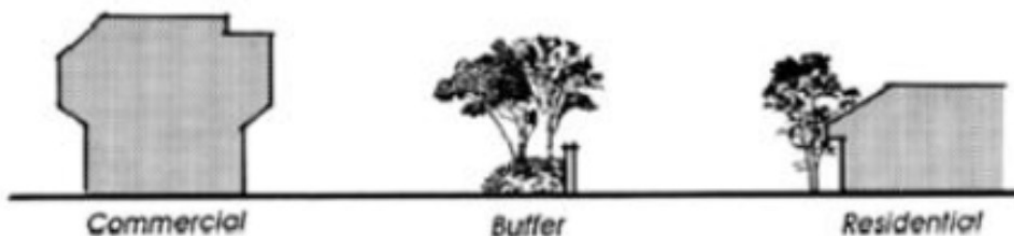
Buffers & Screening

Objectives

Buffering or screening will be required in certain areas to ensure compatibility between incompatible land uses, particularly between commercial, industrial and residential properties. Plantings, earth berms, stone walls, grade changes, fences, distance, and other means can be used effectively to create the necessary visual and psychological separation.

Design Guidelines

- **Appropriateness.** The selection of the proper type of buffer should result from a thorough understanding of existing site conditions, distances to property lines, the intensity of the proposed land use, and the degree of concern expressed by the commissions and boards as well as abutting landowners.
- **Design.** Buffers and screens should be considered an integral part of the site plan. Stone walls, plantings, fencing, landforms, etc., used for buffers should be similar in form, texture, scale, and appearance to other landscape elements.
- **Maintenance.** Buffers should be maintained throughout the life of the project in a condition that assures continual year-round effectiveness. Where plantings do not survive, or grow to a point where they no longer serve as effective buffers, they should be replaced to meet the intent of the approved plan. Walls, fencing, or earth berms used to screen parking lots and add visual interest to a planter strip should be designed as an integral part of the grading plan.
- **Fencing.** Where fencing or other architectural elements (e.g., screening walls) are installed in a highly visible location, they should be treated as an integrated architectural element, complementing the form, style, color, or detailing of the adjacent building.
- **Walls.** Where freestanding walls are installed they should be constructed using durable materials. Walls at the street view should relate to the form, texture, and style of the walls approved by the Town.
- **Combinations.** Combining plantings, berms, fencing, and walls will often result in an economical, attractive way to meet the buffer requirements and create a distinctive landscape.



Variable height fence and vegetation provide good visual separation between commercial and residential neighborhood. The buffer is attractive on both sides sized to accommodate the dumpster for the facility

Stormwater Management

Objectives

To comply with applicable stormwater management requirements, treatment basins, infiltration basins, rain ponds, or other measures will likely be required to maintain the quantity and quality of stormwater runoff. All stormwater management areas should be treated as integral and attractive parts of the landscape.

Design Guidelines

- **Location.** Where stormwater treatment basins or other related facilities are required, they should be graded to conform to natural contours and planted to integrate them into the natural landscape.
- **Design.** Stormwater treatment basins should generally be patterned after naturalistic landforms, avoiding hard geometric shapes, but may be included in open space calculations. Side slopes should be landscaped with appropriate plantings to reduce erosion and screen the basin.
- **Grading.** Abrupt changes in grades and steep side slopes (steeper than 3:1) should be avoided. Transitional grading should be used to blend all earthworks into the natural contours of the land where possible.
- **Structures.** Man-made drainage structures (e.g., culverts, manholes, and outfalls) that are visible from roadways or residential neighborhoods should be screened with vegetation.
- **Shared Basins.** Wherever appropriate, storm water basins (both detention and retention) should be designed to be shared by abutting properties to minimize the amount of land area devoted to stormwater management.



Stormwater management facilities can be designed to create attractive focal points in landscape.

On Site Amenities

Objectives

An attractive public realm is a fundamental ingredient in the success of a commercial development with a high degree of visitors, especially in mixed use development situations. Open air and semi-enclosed spaces that allow people to congregate and interact away from the flow of traffic are important elements of good urban design and should be encouraged in new developments, or in developments with new building(s) or additions.

The different types of open air and semi-enclosed spaces can be categorized as follows:

- **Public Spaces:** Public spaces are areas where the property is owned by a public agency and the public is allowed to enter and congregate.
- **Semi-Public Spaces:** Semi-public spaces consist of areas where the public is allowed to enter and congregate but, unlike public spaces, are owned by a private interest.
- **Private Spaces:** Private spaces are owned by a private interest for the use of adjacent building employees, tenants, or customers. The typology of such spaces may be categorized as follows:
 - **Patio or Plaza Area.** Patio or plaza areas shall be comprised of seating areas provided such patio or plaza has a minimum depth and width of ten (10) feet, and a minimum total area of three hundred (300) square feet.
 - Asphalt is prohibited as a paver; use of decorative pavers or textured, colored concrete is required, and porous materials are favored.

- Patios and plazas should include pedestrian amenities intended to support these places as gathering areas.

- **Landscaped Mini-Parks, Squares, or Greens.** Such park or green areas shall have a minimum depth and width of ten (10) feet and a minimum total area of six hundred fifty (650) square feet, and should include pedestrian amenities intended to support these places as gathering areas.
- **Water Feature.** Water features (e.g. fountain), provided the feature is easily accessed by pedestrians and includes or integrates seating areas for pedestrians.



Example of a shared public space integrated into site planning

- **Outdoor Public Art.** Outdoor public art, provided the feature is visible to pedestrians or motorists.
- **Other.** Other well-designed areas and/or focal feature may be considered, which the plan commissions and boards find consistent with the intent of these guidelines, substantially enhances the development, and serves as a gathering place for residents, visitors, customers, and employees.

Design Guidelines

- **Size.** Patios, plazas, mini-parks, squares and greens should be proportionate in size to the development.
- **Visibility.** In order to serve as a focal point, a feature should be visible and easily recognizable as an area that encourages outdoor assembly.
- **Pedestrians.** Pedestrian amenities for patios and plazas, and for landscaped mini-parks, squares or greens may include seating, lighting, special paving, planting, food and flower vendors, and artwork.
- **Nearby Areas.** The presence or absence of complementary pedestrian spaces in adjacent and surrounding parcels, as well as nearby residential areas, should be considered when determining the appropriate location of an outdoor space and/or feature. Open spaces should be designed and sited to minimize any potential negative impact on adjoining properties, and used in a way that does not create disturbances.
- **Visibility.** Providing good public visibility of on-site outdoor amenities should serve to enhance the security of pedestrians. Accordingly, when a building will be adjacent to a pedestrian plaza, patio, mini-park, square or green, the building wall facing such outdoor amenity should contain at least one of the following elements:
 - A building entry
 - Windows facing onto the outdoor amenity
 - Arcades along the edges of the outdoor amenity
 - Outdoor seating areas
- **Lighting.** For safety, nighttime use, to highlight selected elements and comfort, in compliance with the Town Dark Skies Code.



III. Architecture

Introduction

These Guidelines establish standards for new or renovated commercial buildings that will embrace future design. The Guidelines are not intended to dictate building styles; rather they provide a guide that illustrates the Town's heritage and vision for its future.

These guidelines are intended to supplement, illustrate, and amplify various sections of the existing codes and plans which may set forth a unique identity or style of development for the subject areas.

Architectural Goals:

- Well-designed buildings that reinforce the Town's sense of place, and/or that of the surrounding area.
- Building designs that thoughtfully consider scale, form, orientation, height, setback, massing, materials, color, and architectural features.
- Buildings that present a 'front door' to the street and make a positive contribution to the streetscape.
- Buildings that are designed to address human scale, comfort, enjoyment, and safety of the users.
- Buildings that are designed as permanent, positive additions to the community, constructed of high quality, long lasting materials.
- Street corners that are treated as special places.
- Architecture that recognizes diversity of zoning districts and geographic areas.
- Sustainable design should be a key consideration in building design.

General Architectural Principles

Objectives

The purpose of these Guidelines is to encourage design that provides lasting value and cohesive architectural vocabulary characteristic of the designated District in which redevelopment and future development resides. Building design should be developed to a human scale through careful consideration of architectural forms, massing, detailing, number and use of materials, and color.

Design Guidelines

- **Design.** New buildings should be designed to fit the specific characteristics of their particular site and surrounding area. The architecture will be influenced by use of lasting materials, the specific needs of the intended users, the nature of the intended use, and other site-specific factors.
- **Human Scale.** Buildings and site elements should be designed and detailed to human scale. Many architectural elements can add scale to a building – recessed openings, divided pane windows, building mounted light fixtures, projecting rooflines, covered walkways, and similar features – provided they are designed as integral parts of the overall structure.
- **Freestanding Accessory Structures.** Where freestanding non-habitable structures are allowed (e.g., ATMs, storage units, recycling sheds, trash enclosures, utility buildings), they should meet the same design standards as the principal building(s) on the site. The design of freestanding structures should be coordinated with the principal building through repetition of architectural forms, materials, colors, and detailing.



Renovations & Additions

Objectives

Renovations or additions offer an opportunity to add visual interest to existing buildings, update aesthetics to current guidelines and to strengthen their relationship with the site and nearby structures. The Town expects high quality architectural and site design for all renovations and additions.

Design Guidelines

- **Materials.** Where the existing building currently meets the design guidelines, proposed renovations should be designed to respect the proportions and details of the original building. Where the existing building does not meet the design guidelines, the owner is strongly encouraged to upgrade the most visible portions of the entire structure.
- **Design.** Applications to the Town that involve renovations and additions to existing structures should show all improvements and how they incorporate the appropriate portions of these Guidelines.
- **Architectural Features.** Renovations should retain any distinctive architectural features, which should be incorporated into the addition where possible.
- **Addition Locations.** Efforts should be taken to provide building additions that provide a greater connection towards public roadways and help improve the pedestrian orientation of development.



Façade Design

Objectives

All buildings should present an inviting, human scale facade to the street, internal drives, parking areas, and surrounding neighborhoods. Entrances should be clearly visible from the street where appropriate and reinforced through site and architectural features.

Design Guidelines

- **Front Elevation.** The front facade (the facade facing streets) should be designed as the front of the building. The front elevation should contain a front door, and/or windows, and should incorporate human-scale detailing through the use of cornices, or other projections and details, structural or architectural bays, recessed windows or doors, material or material module changes, or color and/or texture differences so as to be easily recognized as the main access point. On corner lots, the main entrance should face the major street, or be located on the corner of the building. Building entrances should be visible from the street and provide unobstructed areas for pedestrians.
- **Side & Rear Elevations.** Similar materials and detailing, to a lesser extent, should be used on all facades to ensure continuity and design completeness and to give the building scale and visual interest.
- **Entrances.** Each building should have a clearly defined, highly visible customer entrance, which is visually obvious and should be emphasized through the use of such architectural treatments. Building entrances shall be located where a sidewalk exists to a roadway, and in the case of multi-tenant buildings, each separate space should have its own public entrance. The use of the following architectural elements is recommended to add scale to the building, provided that they are integral to the design:
 - Canopies and covered walkways or arcades
 - Arches
 - Differing colors
 - Overhanging rooflines to provide shelter for pedestrians
 - Recesses or projections in keeping with the scale of the building
 - Raised corniced parapets over entrances
 - Gables and dormers
 - Outdoor sitting or dining area
 - Display windows that are visible from the sidewalk
 - Architectural details such as moldings which are integrated into the building design
 - Other features which are designed to add scale and visual interest to the façade
- **Integration into the Design.** Architectural details should be an integral part of the design of the structure, and not merely appendages.
- **Blank Walls.** Facades should not extend for more than 40 horizontal feet in length without incorporating architectural features such as windows, cornices, porches, corners, projections, changes in color or graphical patterns, variety in texture or building materials or offsets. Projections used to break up the mass of the building should extend to the ground. Blank walls should not face roadways, residential areas, or other public viewpoints.
- **Site Design.** Signage, lighting, landscaping and other exterior elements should be designed to complement the facade, avoid visual or functional conflicts, maintain visibility, and create visual interest in ways that are compatible with the architectural character of the surrounding area.

- **Ground Floors.** The horizontal length of the façade of the ground floor of buildings facing public streets should include awnings, transparent display windows, entry awnings, or other similar pedestrian-friendly features, and weather protection elements should be complementary to the building's design. As an alternative, other architectural elements may be used to provide scale and visual interest to the front façade.
- **Shutters.** Where shutters are used, they should be sized to fit the openings and provided for all windows on a given wall.
- **Functional Elements.** All vents, downspouts, electrical conduits, service meters, HVAC equipment, service areas, loading docks, service connections, and other functional elements of the building should be treated as integral parts of the design. Meters, utility banks, HVAC equipment, and other exterior service elements should be contained in service closets, screened with walls or fences, or located out of view from the public. Building elevations should show the location and treatment of all functional elements. The designer is encouraged to locate as many of these functional elements as possible to the side or rear of the building.

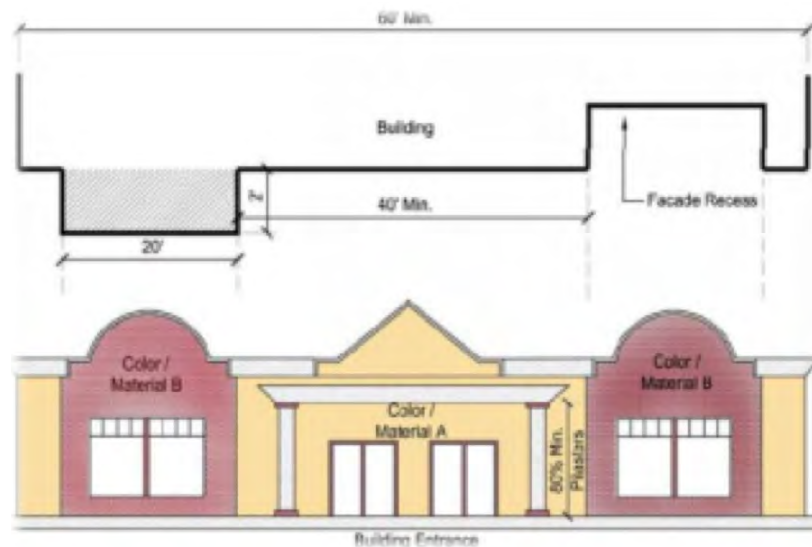


Illustration of how the façade offset may be applied

Building Materials

Objectives

Building materials and design details reflect a building's style and character.

Design Guidelines

- **Materials.** Buildings should be constructed of high-quality materials and the use of variety of materials is encouraged. Acceptable primary materials include brick, clapboards and shingles (wood, fiberglass, metal), and stone or simulated stone. Contemporary secondary or supporting materials with the same visual characteristics as traditional materials (e.g., cement plank clapboards) are acceptable if properly detailed with surface textures and trim at openings, corners, and changes in material and in context with the primary materials. Painted medium density overlay (MDO) plywood is acceptable when used as a secondary material in combination with traditional materials to give it scale. Long-term maintenance needs should be a consideration in the selection of all building materials.
- **Materials Discouraged.** Highly reflective or processed materials (e.g., sheet metal or plastic panels, brushed aluminum, bronzed glass), stucco or synthetic stucco, concrete block, T-111, untreated plywood, particle board, tilt-up concrete panels in general but these may be allowed in the High-Tech Business Park, are all discouraged as the primary facade material.
- **Colors.** Facade colors should be low reflectance. The use of high intensity, high reflectance, chrome, metallic, or fluorescent colors, or black is discouraged as the primary color.
- **Trim.** Where trim is used, it should be painted or stained to complement the building's primary color.

- **Detailing.** Arbitrary changes in materials or embellishments that are not in keeping with the rest of the building are discouraged.
- **EIFS (Exterior Insulation and Finish System).** EIFS is an exterior wall covering that insulates and provides weather protection in a selection of shapes, colors, and textures that can replicate almost any architectural style or finish material, or stand by itself as an architectural finish. In some instances, and with proper maintenance, EIFS may be an acceptable secondary material in building design.



Recommended color palette

Awnings & Canopies

Objectives

When properly installed and maintained, awnings and canopies can enhance the appearance and function of a building by providing shade, shelter, shadow patterns, and visual interest. Where awnings are used, they should complement the design, materials, and color of the building.

Design Guidelines

- **Location.** Where awnings are used, both fixed or retractable, they should be an integral element of the architecture. Awnings should be located directly over windows or doors to provide protection from the elements, and maintained in working condition.
- **Materials.** Awnings and canopies should not be made of highly reflective materials. Their colors should complement the facade of the building.
- **Design Elements.** Graphics used on awnings for identification or advertising are discouraged. If used they should be designed as an integral part of the signage for the property, and be coordinated with other sign elements in terms of typeface, color, and spacing.



Awning act to bring down the scale of the facade

Roofs

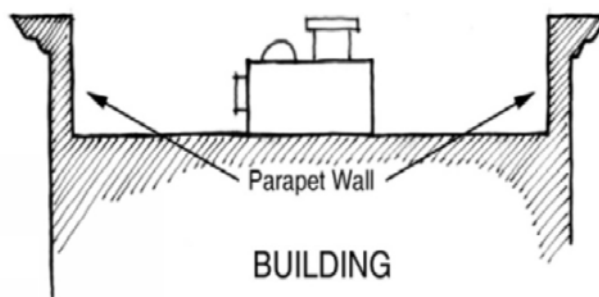
Objectives

Rooflines can add visual interest to the streetscape and establish a sense of continuity between adjacent buildings. When used properly, rooflines can reduce the mass of large structures, emphasize entrances, and provide shade and shelter for the pedestrian.

Design Guidelines

- **Preferred Materials.** Composite asphalt shingles and standing-seam non-glare metal are preferred for visible roofing. High gloss roofing materials are prohibited.
- **Roof Colors.** Roofing materials should complement the color and texture of the building's facade. Roof colors should be muted earth tones or a color that is darker than the facade. Stripes and patterns on the roof are strongly discouraged.
- **Roof Pitch.** Prominent roofs should have a minimum pitch of 4/12 (ratio of rise to run), unless demonstrated to the Town's satisfaction that this is not practicable from an engineering or technical standpoint.
- **Rooflines.** Where appropriate, eaves and roof overhangs should be incorporated into the design of the roof to provide a distinct shadow line.
- **Flat Roofs.** Flat roofs, though discouraged, are permitted except that where any non-architectural roofing materials (e.g. tar and paper) are utilized, such roofing shall be concealed with parapet walls that have 3-dimensional cornice treatments or similar screening methods. All roof-based equipment shall be located on the rear of elevations so as to have minimal visual impact from a public street or surrounding residential uses.

- **Roof-Mounted Equipment.** Mechanical, HVAC, and other equipment mounted on rooftops should be screened from public view or grouped in a location where visibility is limited. Screening for roof-mounted equipment should be designed as an integral part of the architecture to complement the building's mass and appearance.
- **Projections.** The use of cupolas, dormers, chimneys, and other roof projections is encouraged, provided they are designed as integral parts of the structure and do not appear to be floating or pasted on.



Roof mounted mechanical equipment can be effectively screened by roof structure (left). The image at the right illustrates what is hidden beyond a roofline.



Street Corners

Objectives

Buildings located on corners are particularly important because they help define the character of two streets. These high-visibility locations should be emphasized by quality architecture and site development.

Design Guidelines

- **Siting on Corner Lots.** A building on the corner of two public streets should be located as close to the intersection as allowed by the applicable codes. Minimal parking, vehicular travelways, or service areas should be located between the building and property lines along both streets.
- **Corner Buildings.** Buildings on corners should be articulated to add mass and visual prominence to the street corner.
- **Entrance.** The main entrance to the building may be located on the major street or on the corner and designed to be visible from both streets. The architectural treatment of the corner should emphasize its prominent position. This can be accomplished by greater massing, unique detailing, lighting, etc.
- **Focal Points.** Corner locations offer opportunities to create dynamic focal points in the streetscape. These can take the form of distinctive architectural elements, signs, sculpture, lighting, or landscaping. Where they are used, focal points should be visually related to the building as a whole, providing an accent without overwhelming it.



Linear Commercial Buildings

Objectives

Linear commercial buildings (e.g., strip shopping centers, multi-tenant offices, and commercial buildings) should be designed with facade and roofline elements that reduce their scale and add architectural interest.

Design Guidelines

- **Design.** Buildings with multiple storefronts (e.g., strip shopping centers, one story office buildings) should be visually unified through the use of complementary architectural forms, similar materials and colors, consistent details, and coordinated signage. Variations in the front setbacks, especially those projecting towards the street, are strongly encouraged to add visual interest, and create spaces for common entries.
- **Scale.** Linear structures should include architectural elements designed to provide shelter, encourage pedestrian movement, and visually unite the building. These can include covered walkways, open colonnades, and similar features.
- **Entrances.** Pedestrian entrances to each building should be clearly delineated to convey a sense of individuality. This can be accomplished by architectural detailing, roofline breaks, landscaping, lighting, or a combination of these elements. Where covered walkways are used, they should extend the full length of the facade.
- **Roof Lines.** Variations in rooflines, detailing, and building heights should be included to break up the scale of connected linear buildings.
- **Focal Points.** Linear commercial buildings should include a focal point – such as raised entrance way, clock tower, or other architectural elements – to add visual interest, help reduce the scale of the building, and highlight the entrance.
- **Pedestrian Access.** Where a multi-tenant building greater than two hundred (200) feet wide separates two public areas, pedestrian access should be provided through the building(s). The pedestrian pass-through must stay open, regardless of whether businesses are open or closed.





Service Stations & Convenience Stores

Objectives

Service stations and convenience stores that sell gasoline should be designed with facade and roofline elements that reduce their scale and add architectural interest consistent with the design vocabulary of adjacent buildings within the design district.

Design Guidelines

- **Orientation.** Service stations and convenience stores should be sited to face the street.
- **Canopies.** Where canopies are used over gasoline pumps, they should be integrated into the design of the building. Canopies should complement the main structure through consistency in roof pitch, architectural detailing, materials, and color. Pitched roofs with fascia trim are preferred for canopies. Bands of bold color on the canopy and backlighting inside the canopy are discouraged. Consideration to sharing road frontage with the principal structure should be given, depending on location, as has been done in Waunakee along Century Avenue between Woodland Drive and Water Wheel Drive.
- **Pedestrian Circulation.** Connections to the public sidewalk should be included in the site plan to encourage pedestrian use. Access routes leading to or from service stations and convenience stores should minimize conflicts with pedestrian circulation.



Drive-Throughs

Objectives

Drive-throughs (for restaurants, pharmacies, banks, and similar uses) should be subordinate to the design of the main building. Drive-throughs require careful consideration of architectural design and circulation planning to integrate them into the streetscape.

Design Guidelines

- **Drive-Throughs.** Where drive-throughs are allowed, they should be incorporated into the design of the building through their scale, color, detailing, massing, and other architectural treatments. Drive-through operations and other automobile-oriented facilities should be designed with facade and roofline elements through roof pitch, architectural detailing, materials, and color, which reduce their scale and add architectural interest. Bands of bold color on the canopy and backlighting inside the canopy are discouraged.
- **Location.** Drive-throughs should be located at the side or rear of the building and avoid facing public or private roadways. Where drive-throughs are located at the rear, consideration should be given to making the site as visible as possible to ensure the safety of the patrons.



Drive-throughs designed as an integral part of the building. Repeats rooflines, forms and materials used in the main building



Multi Story / “Big Box” Buildings (Significant Enterprises Or Anchor Store)

Objectives

Large scale multi story or “big box” buildings that are typically built of masonry or concrete block materials should include architectural variations and details that provide variety in materials, forms and colors.

Architectural design should add to community character, while providing flexibility to avoid rigid uniformity of design. All elements including the scale and mass of buildings, materials, colors, roof styles, door and window openings, and details should promote a cohesive design aesthetic. Building masses should respond to a human scale with materials and details that provide visual interest at the street and sidewalk level. Buildings should be reduced in apparent mass or articulated to avoid large monolithic shapes.

All buildings need to comply with size regulations and applicable codes. Such structures are generally discouraged due to scale in these Districts, however if designed properly and to an acceptable scale, these may be considered.

Design Guidelines

- **Materials.** Buildings should be constructed of high quality materials that relate to the color, form, and texture of the proposed structure as well as nearby structures.
- **Building Mass, Forms, and Pedestrian Scale.** Variations in facade elements should reduce perceived mass and scale. Variations in color, materials, and/or texture, and a facade composition that uses rhythms and patterns of windows, columns, and other architectural features are encouraged. Buildings should have features and patterns that provide visual interest at the scale of the pedestrian, which reduces apparent mass and that relate to local architectural character.



- **Design Elements.** Moldings and trim should be incorporated into the façade. Building entrances shall contrast with the surrounding wall planes by changing materials and color from the primary façade. Any wall along a street or other area with public should incorporate significant architectural treatments and features to diminish the building mass.
- **Roof Lines and Roof Elements.** Roofs should contribute to the unified appearance of each development and should be considered as seen from ground level, other adjacent buildings and public roadways. Roof lines include the main building as well as entrances, arcades, and porches. Avoid roof/parapet lines running in continuous planes absent variations in height, vertical planes (jogs), or materials. All mechanical, and electronic equipment attached to or mounted on the building roof should be set back from the edge of roof and screened from public view. Screening material should be compatible with materials and colors.



IV. Landscape

Introduction

Landscaping should be an integral part of all site plan developments. Trees, shrubs, and other landscape elements can be used to accentuate buildings, create a sense of identity, reduce the amount of impervious surfaces, and provide human scale. Applicants should carefully evaluate the physical characteristics of each site and their own maintenance abilities when making the final selection to ensure that the plantings will survive and achieve maturity in their selected locations.

These Guidelines are intended to supplement, illustrate, and amplify the existing landscaping standards and the landscaping criteria outlined in the Westport regulations.

Landscaping Goals:

- Incorporate appropriate plantings that are in scale with their surroundings.
- Separate roadways from commercial development by attractive landscape planter strips.
- Incorporate plantings in parking lots to add aesthetic value, reduce their scale, provide canopy shade, reduce radiant heat from the surface, reduce headlight glare, and add seasonal interest.
- Preserve mature trees and other significant landscape features which help define the character of the community.
- Provide screening for less attractive parts of a site or incompatible land uses.
- Help define areas where pedestrians are safely separated from a road or drive pattern.
- Reinforce wayfinding by emphasizing entrances and circulation patterns.



General Landscape Principles

Objectives

Development in Westport Town should be characterized by a rich variety of native landscape materials that enhance human scale, complement the architecture, reinforce circulation paths, highlight entrances, provide canopy shade, and add seasonal interest.

Design Guidelines

- **Plans.** Landscape Plans should be prepared by a landscape architect registered in Wisconsin, or other qualified professional familiar with local growing conditions.
- **Coordination with Site Features.** The landscape plan should show all utilities, signage, lighting, pedestrian circulation, and other site features that may influence the selection or location of plantings. The plan should be designed to avoid conflicts (both at the time of planting and in the future) between plantings and other site elements.
- **Safety.** The selection of plant materials should consider public health so they will not create unsafe conditions, interfere with utilities or block sight lines for pedestrians, bicyclists, or motorists.
- **Rocks.** Large rocks should be used very sparingly as landscape elements and only as accents in mass plantings. Rocks should not be used as substitutions for shrubs. Where used, they should be buried by a third to half of their depth.
- **Variety.** Plant materials should exhibit some seasonal color and interesting texture to create a distinctive, yet low maintenance environment. Landscape plans should strike a balance between monoculture (the use of a single species) and excessive variety.
- **Irrigation.** Underground irrigation is encouraged in front setbacks, public spaces, and other highly visible areas. It should be designed to prevent overflow or flooding onto walkways or parking lots.
- **Invasive Plants.** Invasive plants and plants not naturally occurring should not be utilized. Plantings should be confirmed with Dane County Parks of the DNR for consistency with its prairie plantings. These plants include trees, bushes, shrubbery and other ground plantings, including the eliminations of plants such as buckthorn and honeysuckle.

Tree Protection

Objectives

Mature trees along roadways in the Town and nearby areas are an important element of community character that also reflects the Town's preservation initiatives. They provide significant shade, year-round visual interest, and comfort to residents. Where practical, existing mature specimen trees should be preserved during development. Preserving large existing trees within the planting strip will decrease the number of new trees required. Plans should be written by a certified arborist or landscape architect to comply with the Town Tree Protection Code.

Design Guidelines

- **Existing Trees/Plants.** The preservation of existing or unique trees or other significant plantings should be considered during the initial site inventory and development of the sketch plan. Transplanting and reusing trees and other plantings is strongly encouraged.
- **Tree Protection.** The landscape plan should show how existing trees and vegetation will be protected during construction. As a general rule, no construction activity should be allowed within the drip line (outer edge of the tree canopy). This includes grading, compaction, utility installation, stockpiling of construction material, or movement of vehicles.
- **Temporary Measures.** Barricades in the form of snow fencing or similar materials should be installed during construction to protect trees and their root zones.
- **Grade Changes.** Grading within the drip line should be avoided since it may cause irreparable damage to the root system and cause the tree to die.
- **Tree Walls/Wells.** Where grading is required near trees to be preserved, properly designed tree wells or walls may be used to ensure the long-term health of the tree.



Planting Strips

Objectives

Commercial development should be separated from the adjacent roads by landscaped planting strips. These areas should be designed to screen parking areas, separate land uses, and visually unify the Town's business districts.

Design Guidelines

- **Ground Covers.** Appropriate groundcover includes turf grass, ornamental grasses, perennials, low-growing evergreens and flowering shrubs. Planting other than turf grass should be spaced close enough to achieve full coverage within 3 years after installation.
- **Plant Masses.** Shrubs, perennials, annuals, and ornamental grasses used in planter strips should be installed in masses or 'drifts' that emphasize colors, forms, and textures.
- **Street Side Trees.** The required trees within planter strips may be installed in a linear fashion or informal groupings. Linear plantings may be appropriate along roadways to create a boulevard effect, using large spreading deciduous trees to define the edge of the travelway, provide shade and add scale, a sense of place, and orientation to commercial corridors. Roadside Plantings. Trees and other landscaping planted at intersections should preserve a clear area for sight lines to roadways and businesses.
- **Parking Lots.** Parking areas should be separated from the street by plantings, earth berms, walls, and/or other landscape elements to minimize headlight glare and the view of vehicles, while still allowing the public to see the building.



Parking Lot Landscaping

Objectives

Landscaping in parking lots can be used to improve their appearance, reduce the scale and amount of paved areas, define edges, provide shade, reduce headlight glare, and add seasonal interest.

Design Guidelines

- **Trees in Parking Lots.** The interior area of any parking lot should be landscaped, with planting requirements set forth in applicable codes. The development plan should incorporate landscaped islands as a means of creating an attractive character; establish a sense of place, and to increase the value and marketability of the development.
- **Location of Trees.** Trees should be planted a minimum of three feet (3') from the end of parking lot islands.
- **Safety.** Trees in parking lots or those that abut walkways should be pruned above the paved surface to avoid becoming an obstacle. Shrubs and ornamental plantings in parking lot islands should not block visibility.



- **Entryways.** The design of entryways should provide for a substantial landscape treatment. A variety of plant materials should be used to establish an attractive landscape with year-round color and texture. In addition, other elements such as earth berms, decorative walls, low fencing, landscape lighting, sculptural elements, paving, water feature, and signage may be utilized based on an effective design and these themes should be carried through the development.



Tree Selection & Planting

Objectives

Trees are used throughout the Town, including those planted within the right of way, near buildings, and in parking lots. Trees should be sited to achieve full maturity and display their natural form, and according to applicable codes.

Design Guidelines

- **Suitability.** Trees should be resistant to insect infestation, drought, disease, roadside salt, and auto emissions. All plant material should be suitable to the Town's growing conditions.
- **Planting Locations.** Trees should be planted in locations where their root development and branching patterns will not interfere with window displays, signage, underground or overhead utilities, streets, and sidewalks.
- **Pedestrian Movement.** The lower branches of trees planted near pathways and sidewalks should minimize interference with pedestrian movement throughout the year.



Trees, shrubs and perennial ground cover used to create a highly unified, inviting streetscape

Shrubs & Ornamental Planting

Objectives

A variety of shrubs and ornamental plantings should be used throughout the development to add seasonal color, provide visual interest, help define spaces, screen undesirable elements, and emphasize circulation routes, and in accordance with applicable codes.

Design Guidelines

- **Variety in Plantings.** The use of flowering shrubs, evergreen shrubs, perennials, annuals, vines, ornamental grasses, and other plant material is highly recommended, in addition to street trees, evergreen trees, and ornamental trees.
- **Selection.** The selection of plantings should consider ultimate height and spread, maintenance, pest and disease tolerance, and their nuisance potential (severe thorns, excessive leaf litter, etc.)
- **Foundation & Wall Plantings.** Planting beds are recommended along exposed building edges, foundations and uninterrupted walls. Plantings should be installed a minimum of 18 inches from the wall to allow proper root zone development. Plantings should provide either a formal pattern or a naturalistic blend of heights, colors, and species.



Small areas of accent plantings can add color, texture and visual interest to the landscape.

Landscape Maintenance

Objectives

Landscape plans should anticipate 3-8 years for shrubs to achieve maturity, and 15-20+ years for trees. Proper maintenance should be provided to assure that the landscaping achieves its proper form and full height. Maintenance of all landscape elements should be considered in the development of the Site Plan.

Design Guidelines

- **Replacement Planting.** If plant materials specified, including grass areas, do not survive or are damaged, they should be timely replaced in accordance with the approved planting plan and to provide the necessary landscape effect.
- **Low Maintenance Materials.** The use of plant materials and landscape elements that require a low degree of maintenance is strongly encouraged.



V. Lighting



Introduction

Outdoor lighting directly impacts the visual appearance of the Town, as well as the Town's safety and security. The following lighting guidelines are designed to help balance the need for visibility and safety and enhance the visual quality of the Town, while respecting the privacy of abutting residential properties.

Lighting in commercial developments is a major determinant of night time activity. It should create a sense of safety, particularly for pedestrians, and should emphasize key features of the site. At the same time, it needs to balance the lighting needs of the different uses on the site and reinforce a unified image and identity for the project.

Lighting plans should consider illumination levels and fixtures that accommodate safety and visibility needs, but are also respectful of neighbors and are compatible with nearby development. Light levels should comply with the Town of Westport's Exterior Lighting Ordinance (Chapter 7 - The Westport Exterior Lighting Code) or other applicable rules, and not exceed the Illuminating Engineering Society of North America (IESNA) recommended minimum standards. These Guidelines are intended to supplement, illustrate, and amplify such provisions.

Lighting Goals:

- Provide appropriate levels of lighting to ensure visibility and safety in both pedestrian and vehicular areas while avoiding over-illumination.
- Promote wise energy consumption.
- Help to unify the quality of the visual environment through the selection of attractive, appropriately scaled fixtures.
- Avoid light fixtures or mountings that can cause distractions or hazards to motorists or pedestrians.
- Eliminate reflected light from parking lots and large commercial users that contribute to skyglow.
- Eliminate intrusions onto abutting properties, especially residential uses.
- Enhance noteworthy features such as monuments, sculpture, or architectural elements.

General Lighting Principles

Objectives

Exterior lighting should be designed to provide the minimum level of illumination necessary for security, safety, and visual appeal for both pedestrians and vehicles, while meeting Dark Sky rules. Lighting should allow activity after sunset without adding to unnecessary skyglow. Functional, aesthetic, and safety goals should be met with fixtures that are designed as integral site elements.

Design Guidelines

- **Lighting Plan.** Lighting plans required for development plan review should be presented with the application to enable staff, the plan commissions, to properly understand and review the lighting design.
- **Pole and Fixture Design.** The location and design of lighting should complement adjacent buildings, pedestrian amenities, and site elements. Poles and fixtures should be proportionate to the buildings and spaces they illuminate.
- **Mounting Heights.** Light fixtures should be mounted at the lowest level allowing compliance with IESNA practices and applicable local codes.
- **Safety and Energy Conservation.** Illumination levels should not exceed the minimums to provide safe conditions as currently defined by the IESNA.



- **Safety Considerations.** The design and placement of plantings, buffers, screen walls, fencing, and other landscape elements should be coordinated with the lighting plan to eliminate dark spots and potential hiding places.
- **Feature Lighting.** Unique building or landscape features may be highlighted if the lighting does not create glare or distraction.
- **Light Pollution.** Lighting should not cause spillover onto neighboring residential properties or create dangerous conditions due to glare on adjacent roadways.
- **Energy Saving Devices.** Wherever practicable, lighting design should include the installation of timers, photo sensors, and other energy saving devices to reduce the overall energy required for the development and eliminate unnecessary lighting. It is generally desired the lighting not be used from 10 p.m. to 5 a.m. unless necessary for safety, or the Town determines lighting can be used during these times as part of a positive design parameter.



Driveways, Parking Lots, Outdoor Sales & Service Areas

Objectives

Proposed lighting for driveways, parking lots, and outdoor sales and service areas should be designed to provide the minimum lighting necessary for traffic and pedestrian safety. Lighting should not cause glare or avoidable spillover onto adjacent properties. Poles and fixtures should be proportional in size to the roadways they are illuminating.

Design Guidelines

- **Illumination.** Driveway lighting should be designed to illuminate the roadway and sidewalk, with a concentration on roadways. Light fixtures should be selected and aimed to prevent glare and spillage onto abutting properties.
- **Design.** The design and color of fixtures (poles and luminaries) used along driveways should complement the architecture, landscaping, and street furnishings of the site to be developed or redeveloped in terms of color, form, and style.
- **Layout.** The alignment and spacing of fixtures in parking lots should follow a regular pattern that is coordinated with the orientation of buildings and other site elements.
- **Location.** Light poles should be incorporated within raised planting areas wherever possible to avoid damage from vehicles and plows.
- **Coordination with Planting Plan.** The lighting plan should be coordinated with the landscape plan to avoid obstructions from large trees, dark spots from shadows, or other conflicts as plantings mature.



Pedestrian Spaces

Objectives

The lighting of pedestrian spaces should consider users' needs and safety. Light standards should adequately, but not excessively, illuminate not only the space occupied by people, but also the elements within those spaces such as stairs, walls, benches, curbs, and landscaping. Light fixtures should be oriented to pedestrian circulation so that pedestrian ways are emphasized and safety is enhanced. Generally, the Town's Dark Skies Code calls for fully shielded and full cut off lighting.

Design Guidelines

- **Heights.** Mounting heights for pedestrian lighting should be appropriate for the project and the setting. Light bollard fixtures, 3-4 feet in height, and ornamental fixtures, up to 12 feet in height, are encouraged as pedestrian area lighting.
- **Luminaries.** Lamps should be high efficiency, housed in a luminaire that is classified by IESNA as a cutoff fixture. LED fixtures are preferred. In general, illumination should not exceed 100 watts.
- **Decorative.** Ornamental and decorative lighting should be used to highlight significant design elements (e.g., gateways, plazas, major building entrances).
- **Scale.** Pedestrian circulation is encouraged and therefore pedestrian-oriented lighting is encouraged. Pedestrian area lighting should emphasize the location of pedestrian ways and be in character with the architectural and landscape design of the development.
- **Number of Fixtures.** For pedestrian circulation areas the use of a greater number of low fixtures is preferred over fewer taller fixtures. In either case, the layout should avoid major dark spots between fixtures.



Building Facades & Landscape Lighting

Objectives

Facade lighting is a way of highlighting special architectural features and attractively landscaped areas, while adding depth and variety to developments at night. Lighting used to illuminate building facades and landscaping should be limited to areas where it enhances particular features in accordance with the overall lighting plan and does not disturb surrounding residential areas.

Design Guidelines

- **Location.** Lighting fixtures should be properly sited, aimed, and shielded so that light is directed only onto the building facade. Lighting fixtures should not be directed toward adjacent streets, sidewalks, or properties.
- **Mounting Heights.** The maximum light fixture height for building-mounted light fixtures should be 15 feet on the facades facing public streets (the front lot line) and 20 feet on all other facades.
- **Wall-Mounted Fixtures.** Facade-mounted lighting fixtures should include full face shielding: either solid panel or louvers that direct the light upward or downward.



Service Stations, Convenience Stores, Service Areas & Canopy Lighting

Objectives

Lit canopies, architectural features, or devices used to illuminate gas stations, convenience stores, and drive-through elements of a building should facilitate the activities taking place in such locations without creating glare onto adjacent properties or roadways.

Design Guidelines

- **Canopy Luminaries.** Canopy-mounted light fixtures must comply with the Town Exterior Lighting Ordinance so motorists cannot see the source of light. Drop fixtures are not permitted.
- **Fascia.** Lights should not be mounted on the sides (fascia) or top of the canopy. Sides and tops of canopies should not be illuminated. Internal calm sign lighting has been allowed in the past as illustrated below.
- **Service Areas.** Fully shielded lighting fixtures should be used in all parking areas, in service and delivery areas.



VI. Signage

Introduction

Signs play a central role in providing information and wayfinding. They inform motorists, bicyclists, and pedestrians, while having a direct effect on the overall appearance of the roadway.

These Guidelines are intended to supplement, illustrate and amplify the applicable sign code provisions. Other elements found in adopted plans for the Town.

Landscaping Goals:

- Provide basic, legible information with attractive, highly legible signage.
- Create distinctive signage that is compatible with quality architecture and site design.
- Reduce visual clutter along roadways in the Town.
- Protect the investment of commercial interests throughout the Town by establishing a quality benchmark for future signage.
- Promote safety and wayfinding by ensuring adequate display of building/business address number.



Free standing / monument signage is encouraged to reflect the architecture and building materials used in the primary structure.

General Sign Principles

Objectives

Commercial establishments should be identified by attractive, legible signs that serve the needs of the individual business, complement the site and the architecture, and are legible to both the motorist and pedestrian.

Design Guidelines

- **Signage Plan.** Information on the location and design of signs should be submitted as part of the application. The applicant should resubmit the plan to the planning staff for review, if the building's tenant is unknown at the time of application.
- **Compatibility.** Signs should be designed to achieve a high level of visual compatibility with the building(s) and surroundings through the use of similar detailing, form, color, lighting, and materials.
- **Design.** The shape of the sign should complement the architectural features on the building. Simple geometric shapes are preferred for all signage. Signs should be detailed to complement the building.
- **Lettering Size.** In general, the minimum lettering size for identification signs should be six inches in height. Smaller letters are generally unreadable at high speeds and may require motorists to slow down to read them, potentially causing safety hazards.
- **Advertising Features.** Objects other than signs designed primarily to attract public attention are discouraged because they distract motorists and contribute to visual clutter. These include greater-than-life size models of food or other products, replicas of spokes-people associated with commercial products and rows of flags and banners.
- **Materials.** The composition of signs shall be made of durable materials that reflect those used on the principal structure. The use of painted plywood shall be discouraged.
- **Messages.** Signs used to identify businesses should be kept simple and direct in message and content and convey only the most essential information about the business.

Freestanding Signage

Objectives

Signage that is not affixed to a facade shall be designed to complement the design of the building and in concert with the signage pattern and character of public and private development.

Design Guidelines

- **Height.** Signage is encouraged to be erected at lower heights, maintaining clearance above landscaping and parked automobiles, and below power lines and mature trees.
- **Signage Support Structures.** The use ground mounted signs are generally encouraged, as opposed to pole signs. Support structure for such signs shall be of materials that are compatible with the sign and surrounding site, preferably constructed with a stone base.
- **Readerboards.** Where readerboards are part of a permanent sign, they should contain no more than three lines of text. Lettering height should be a maximum of 6". The readerboard should be fully integrated into the overall sign design by virtue of its form, scale, color, and detailing. Due to driving distractions caused by these boards, their use is discouraged.
- **Appropriate Landscaping.** Landscaping plans in compliance with appropriate codes shall be utilized.



Highly visible characterized by simplicity in materials with prominent display of the building address for motorists, and use materials that match the primary structure.

Building-Mounted Signs

Objectives

Building-mounted signs used to identify commercial properties should be integrated into the design of the building.

Design Guidelines

- **Design.** Facade-mounted signs should be designed as an integral element of the architecture. The shape and materials of the sign should complement the architectural features on the building.
- **Location.** Signs should not be mounted in locations that obscure architectural details on the building. Signage should be mounted on vertical surfaces without projecting above the fascia trim.
- **Signage Placement.** Signage on awnings, windows, and other facade elements shall be designed to complement and be consistent with the building architecture.



These signs are well integrated into the architecture, using only essential information about the tenant

Multi Tenant Properties

Objectives

Multi-tenant commercial properties should provide legible, attractive signs that help people identify the property without contributing to sign clutter. Entrance signs should stress the identity of the place and de-emphasize individual tenants that occupy it.

Design Guidelines

- **Hierarchy of Signs.** A hierarchy of signage should be established to facilitate wayfinding and minimize site clutter. Multi-tenant properties on major roadways should be identified by a simple identification sign in a highly visible location.
- **Identification Signs.** Multi-tenant buildings or multi-building sites should have one identification sign conveying an overall identity for the property. This sign should be located near the main entrance to reinforce circulation patterns and minimize visual clutter. Identification signs that also list multiple tenants should exhibit a logical hierarchy in the display of information (i.e., address, name of building/development, primary tenant, other tenants).
- **Informational & Directional Signs.** Entryway, informational and directional signs should conform to the applicable ordinances, and they should be an integral design element of the development's character and architecture.
- **Compatibility.** The design of multi-tenant signs should be coordinated with the design of the principal building (s) in terms of color, materials, detailing, and style.



This retail center is identified by a single sign at the entrance; names of the tenants are primarily found on the building facades. The result is less clutter on the road.

- **No Separate Monument Signs.** Separate monument signs for individual businesses in a multi-tenant property shall not be allowed. Wall mount signs shall be allowed.
- **Color Consistency.** Multi-tenant signs should conform to a simple color and graphic palette in order to minimize the confusion and clutter of the sign. In general, multi-tenant signs should have no more than three colors.



A multi-tenant sign with clean hierarchy of information. Individual tenants are listed in large print for legibility

Externally-Lit Signs

Objectives

Lighting for externally-lit signs should be designed as an integral part of the sign design. Lighting must not create glare that would distract motorists or pedestrians, nor should the degree of illumination disturb the surrounding residential areas or contribute to light pollution. Externally lit signs are not a preferred option and should generally be discouraged.

Design Guidelines

- **Light Level.** The illumination level on the vertical surface of the sign should be bright enough to provide a noticeable contrast with the surrounding building or landscape without causing undue glare or reflection.
- **Lighting.** Lighting fixtures should be carefully located, aimed, and shielded so that light is directed only onto the sign facade. Lights should not be aimed toward adjacent streets, sidewalks, or abutting properties. Ground-mounted lighting should be screened or partially buried to minimize the view of the light source.
- **Design.** Light fixtures and mounting devices should be selected to complement the color and design of the sign and the architecture. Concealed light sources are strongly encouraged.



Internally-Lit Signs

Objectives

Internally-lit signs should not create glare that would distract motorists or pedestrians, nor should the degree of illumination disturb surrounding residential areas or contribute to light pollution.

Design Guidelines

- **Design.** Internally-lit signs should consist of light lettering and/or symbols set against a dark background to minimize the amount of light emanating from the sign. Internally-lit letters and symbols are preferred over whole panels that are internally lit.
- **Intensity.** Internally-lit signs should not act as light fixtures or cause glare on nearby pathways or roadways.
- **Maintenance.** Signs should be located where they can be easily maintained. Non-functioning bulbs should be replaced immediately.



Effective use of individually-lit letters to create a simple identity for a commercial establishment

Conclusion

This document is to provide a guide for commercial property developers when planning in Westport. Care must be given to integrate design and use into the Town's history and infrastructure. Following the various specifics and suggestions provided here will allow those seeking to locate and develop in the Town the success they are seeking, and still allow the Town to keep its Wisconsin rural hometown feel.

Appendix A

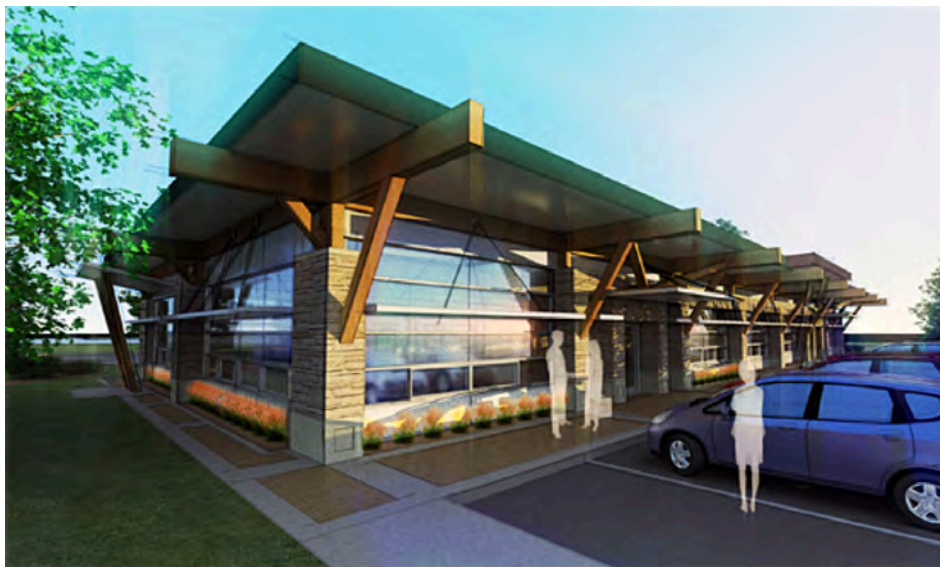
Zoning And Planning Authorities

1. Village of Waunakee and Town of Westport Joint Comprehensive Plan (Including CORP), January 2017
2. City of Middleton/Town of Westport ETZ Area Comprehensive Plan (See 1, above)
3. Village of Waunakee Zoning and Land Division Codes
4. City of Middleton Zoning and Land Division Codes
5. Town of Westport Land Division Code (Title 10, Chapter 2)
6. Town of Westport Building Code, including Design Review Code (Title 10, Chapter 1)
7. Town of Westport Zoning Code (Title 10, Chapter 9)
8. Various Town of Westport Land Use Regulations
 - a. Title 10, Chapter 4, Erosion and Stormwater Runoff Control
 - b. Title 10, Chapter 5, Minimum Housing Code
 - c. Title 10, Chapter 6, Commercial Exterior Maintenance
 - d. Title 10, Chapter 7, Historic Preservation Code
 - e. Title 10, Chapter 8, Forestry Management
9. Town of Westport “Dark Skies” Code (Title 9, Chapter 7, Exterior Lighting)
10. Town Public Utilities Regulations (Title 5, Chapters 1-7)
11. Town Public Works Regulations (Title 4, Chapter 1-5)
12. Dane County Farmland Preservation Plan
13. Dane County Highway Use Regulations
14. Various State Statutes
 - a. Zoning Regulations, Various Sections of Chapters 60, 61, 62 and 66
 - b. Land Division Regulations, Chapter 236

Appendix B

High-Tech Business Park District Architecture and Site Examples





Appendix C

Town Center District Architecture and Site Examples





Appendix D

Westport Commons District Architecture and Site Examples

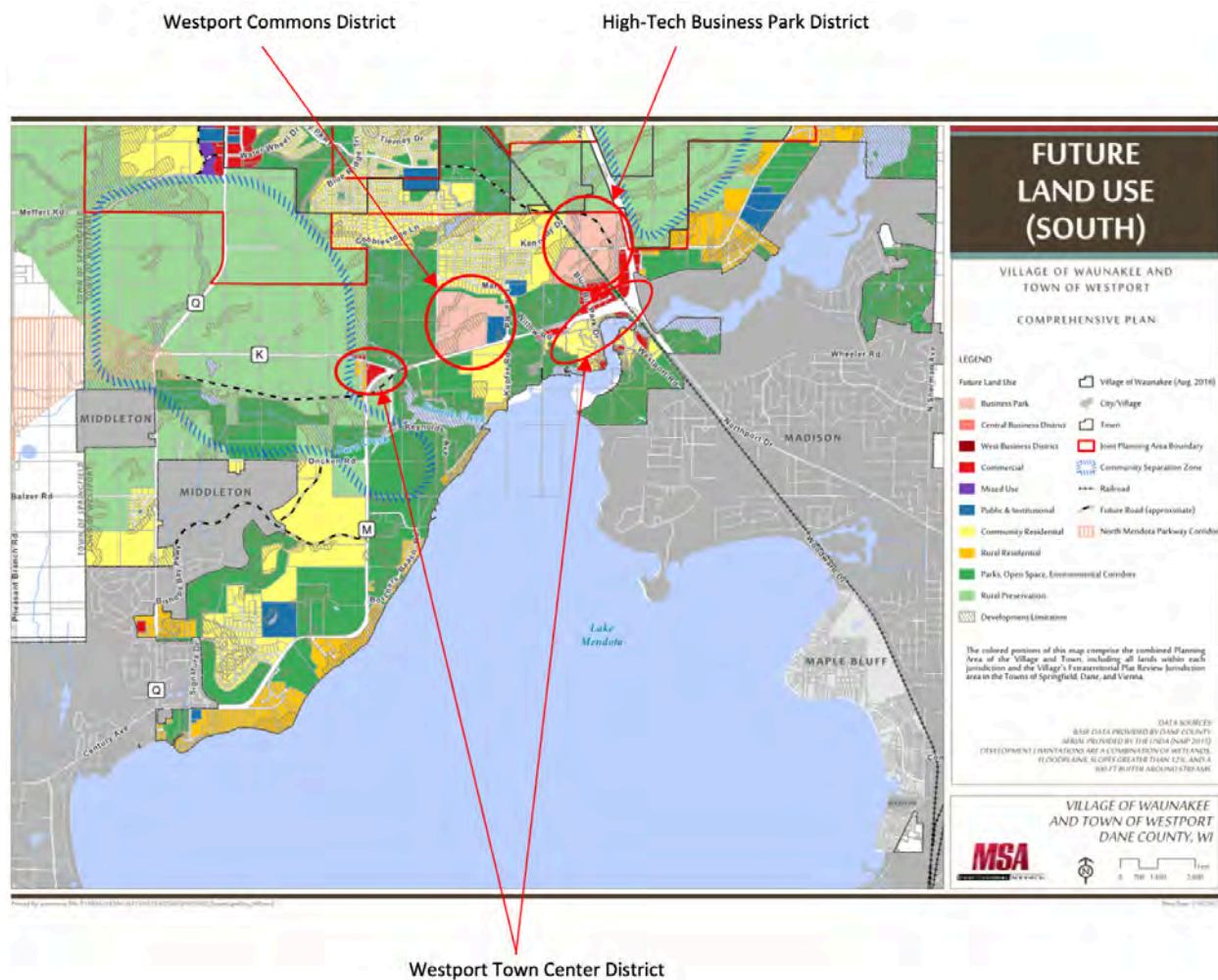




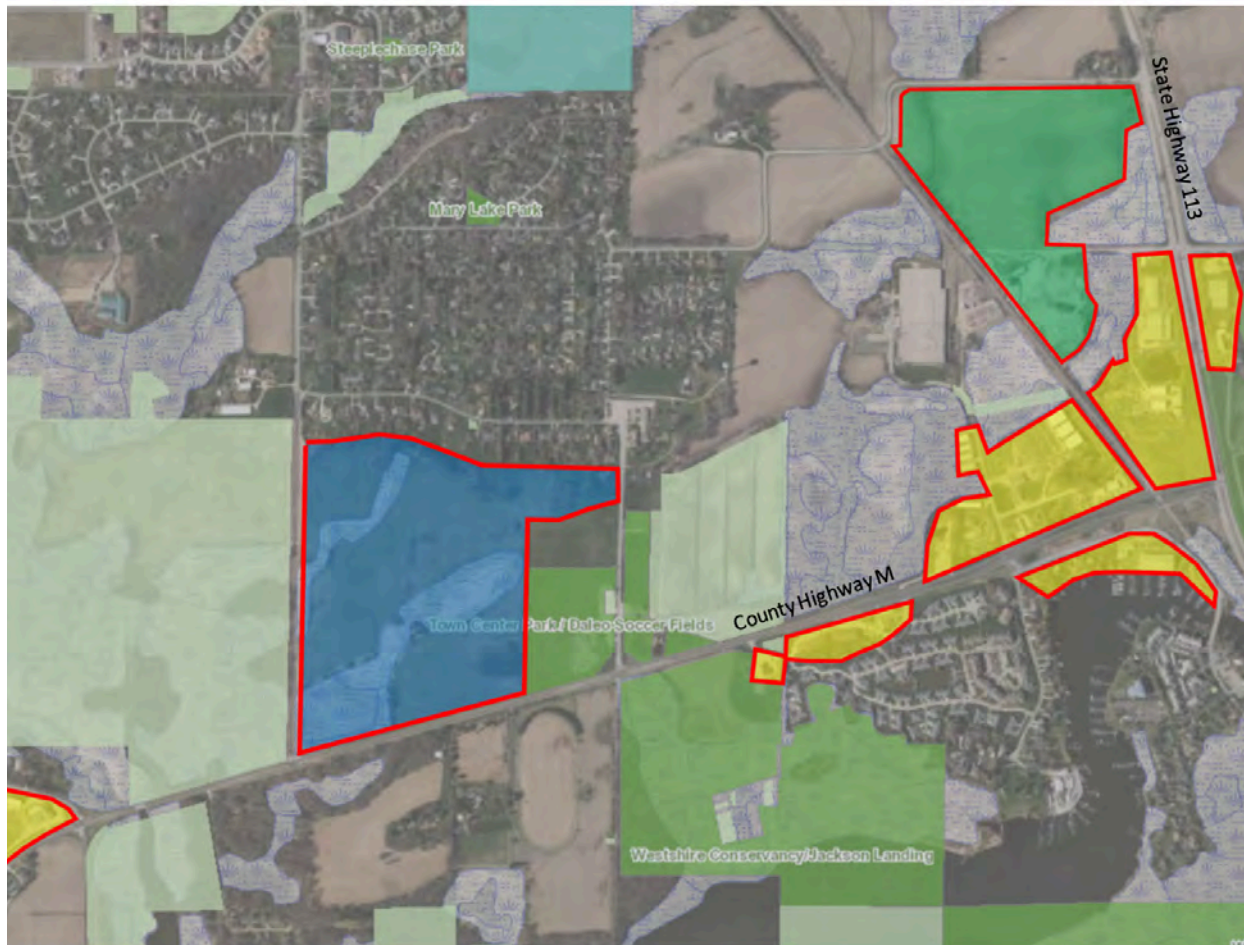




Map 1 – Comprehensive Plan Map Of Relevant Area



Map 2 – Aerial View Of Design Districts



Town of Westport
District Boundary Map

-  High-Tech Business Park
-  Westport Town Center
-  Westport Commons



PLAN COMMISSION SUMMARY SHEET

MEETING DATE: 02/11/2019

ITEM: NEW BUSINESS

TITLE: Status Discussion on Woodland Crest and HyVee Development

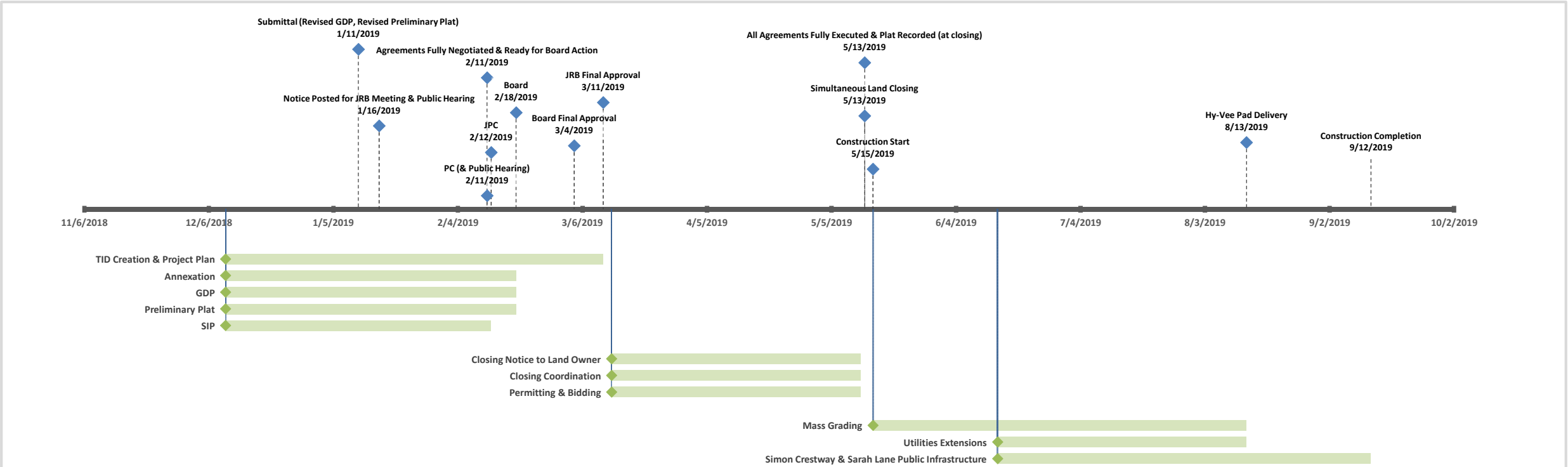
ISSUE SUMMARY: We will provide a status update and proposed schedule going forward with the proposed development

STAFF RECOMMENDATION: None

RECOMMENDED MOTION: No action necessary.

ATTACHMENT(S): Timeline

FOR MORE INFORMATION CONTACT: kevin@waunakee.com or (608) 849-6276



Woodland Crest Project Timeline

Tasks

Start	End	Duration	Task
12/10/2018	3/11/2019	91	TID Creation & Project Plan
12/10/2018	2/18/2019	70	GDP
12/10/2018	2/18/2019	70	Annexation
12/10/2018	2/12/2019	64	SIP
12/10/2018	2/18/2019	70	Preliminary Plat
3/13/2019	5/12/2019	60	Closing Notice to Land Owner
3/13/2019	5/12/2019	60	Closing Coordination
3/13/2019	5/12/2019	60	Permitting & Bidding
5/15/2019	8/13/2019	90	Mass Grading
6/14/2019	8/13/2019	60	Utilities Extensions
6/14/2019	9/12/2019	90	Simon Crestway & Sarah Lane Public Infrastructure
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Milestone Dates

Date	Agenda Items for Approval	Milestone
1/11/2019	N/A	Submittal (Revised GDP, Revised Preliminary Plat)
1/16/2019	N/A	Notice Posted for JRB Meeting & Public Hearing
2/11/2019	Annexation Agreement, Developer's Agreement, Escrow Agreement, TIF Agreement	Agreements Fully Negotiated & Ready for Board Action
2/11/2019	TID & TIF Project Plan, GDP, Preliminary Plat, Annexation, SIP If Needed	PC (& Public Hearing)
2/12/2019	If Needed	JPC
2/18/2019	GDP, Preliminary Plat, Annexation Agreement & Ordinance, Developer's Agreement, Escrow Agreement	Board
3/4/2019	TID & TIF Project Plan, TIF Agreement	Board Final Approval
3/11/2019	TID & TIF Project Plan	JRB Final Approval
5/13/2019	N/A	All Agreements Fully Executed & Plat Recorded (at closing)
5/13/2019	N/A	Simultaneous Land Closing
5/15/2019	N/A	Construction Start
8/13/2019	N/A	Hy-Vee Pad Delivery
9/12/2019	N/A	Construction Completion